

Adopted March 23, 2005
Ordinance No. 4019

MISSION-71 Business Park



SPECIFIC PLAN
Pomona, California
(CZ 04-003/TTM04-006/CUP04-006)
City of Pomona Planning Division
505 South Garey Avenue
Pomona, California 91769

Owner/Developer:

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SPECIFIC PLAN
POMONA, CALIFORNIA

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SECTION ONE
PROJECT INTRODUCTION

I.A AUTHORITY FOR SPECIFIC PLAN APPROVAL

The adoption of this Specific Plan No. CZ 204-003/TTM04-006/CUP04-006 by the City of Pomona is authorized by Section 65450 et seq. of the California Government Code. The Government Code authorizes cities or counties to prepare, adopt and administer specific plans for portions of their jurisdictions, as means of implementing the General Plan.

This Specific Plan has been prepared according to the requirements set forth in the California Government Code section 65451. Adoption of this Specific Plan is being accomplished by ordinance amending the Pomona General Plan to designate the area covered by this Specific Plan as "SP." As part of the project, the Land Use Element of the General Plan is hereby amended to provide for a designation of "Specific Plan," and the Zoning Ordinance is hereby amended to indicate "CZ 204-003" for the Specific Plan area.

I.B PROJECT LOCATION

The Mission – 71 Business Park (“the Project”) is located on approximately 53.61 acres of land, plus approximately 10 acres owned by the Pomona Unified School District in the City of Pomona. The Project is located in the northeast quadrant of the intersection of the Route 71 Corona Expressway and Mission Boulevard, bounded by the Union Pacific Railroad on the north, and the extension of Dudley Street to the East (including the Pomona Unified School District Property). The Project is bisected from east to west by Second Street. A strategic location, the Project serves as the primary western gateway into the City of Pomona.

At the request of the City of Pomona, the scope of this Specific Plan is intended to include the Project, as well as the property immediately east of the Project, east of Curran Place, bounded by an extension of Dudley Street on the east, which is the Pomona Unified School District Property. It should be noted that the Pomona Unified School district has authorized the City to master plan this site as part of the Project.

The 71 Corona Expressway is the primary north-south corridor between the 10 and 60 freeways to the north, and the 91 freeway to the south through the Chino Valley. The Expressway is planned ultimately by Caltrans to be a freeway for its full length from the 71 – 10 – 57 – 210 Kellogg interchange in northwestern Pomona to the 91 interchange in western Corona. Currently, there are a series of at-grade intersections with stoplights remaining in the City of Pomona on the Expressway, including the intersections of the Expressway and Mission Boulevard, and the Expressway and Second Street. All of these are planned to be either closed or replaced with overpasses/underpasses. At the intersection of the 71 Corona Expressway and Mission Boulevard, an overpass by Mission Boulevard, coupled with a lowering of the Expressway several feet below grade, is planned to start construction in 2006. This work will include on- and off-ramps onto the Expressway, which will eliminate the intersection of Second Street onto the Expressway and create a cul-de-sac for Second Street immediately east of the Expressway.

I.B PROJECT LOCATION (continued)

Mission Boulevard is one of several major east-west thoroughfares through the City of Pomona, and the only one to run through the heart of the City, at the Civic Center, currently undergoing a revitalization of commercial and residential uses. It is a primary bus route and connects the City of Pomona with the cities of Diamond Bar, Walnut and Industry to the west. Both the 71 Corona Expressway and Mission Boulevard serve as primary trucking access routes into the Project.

The Union Pacific Railroad right-of-way is a major east-west main-line with frequent use by long-haul and local freight trains, as well as Metrolink commuter passenger trains. Immediately south of the railroad right-of-way, on the northern border of the project, is a 30' wide SCE power line easement with high-voltage towers and transmission lines.

On the east side of the Project is Curran Place, a two-lane local street that provides limited access for trucks and automobiles between Second Street and Mission Boulevard. Currently there is no left-turn access for eastbound traffic from the 71 Corona Expressway across the divided Mission Boulevard directly into the Project. This is provided by making a u-turn at Dudley Street, a signalized intersection to the east of Curran Place. The City of Pomona has required that on or prior to the closure of the Second Street intersection with the Corona 71 Expressway, Curran Place shall be upgraded to a four-lane street; that the interchange of Mission Boulevard and Curran Place shall be upgraded to full flow, including a left-turn lane from eastbound Mission Boulevard onto northbound Curran Place; and that the intersection be prepared for future signalization when traffic needs warrant. The applicant shall be responsible for construction of these upgrades, including acquisition of the necessary right-of-way, from the Pomona Unified School District.

VICINITY MAPS



Exhibit I-B

EXISTING AERIAL VIEW



Exhibit I-C

I.C PROJECT TOPOGRAPHY

The Project site is relatively flat with a minimal slope from north to south and east to west of less than 1%. Drainage is into local storm drain systems located in public streets. There are no unique topographical aspects to the Project.

I.D PROJECT HISTORY

The Project served at one time as the eastern half of the General Dynamics campus (the western half is located west of the 71 Corona Expressway and is not a part of the Project or Specific Plan). When General Dynamics ceased operating the facility, the title was returned to the United States government, who sold the campus to the City of Pomona. The City of Pomona in turn, sold the properties east of the 71 Corona Expressway, north of Mission Boulevard, south of the railroad right-of-way and west of the present Curran Place to Boyd Furniture Group. The property east of Curran Place was sold to the Pomona School District (Note: Curran Place was improved as a result of this transaction).

As a part of the sale by the federal government to the City of Pomona, an environmental covenant was placed on the properties. Due to decades of manufacturing of missiles and other weapons for defense purposes, the soil beneath Building 2, the largest building of the property (approximately 750,000 s.f.) was contaminated. The federal government was willing to indemnify the City of Pomona, and any future Owners of the property, only if the soil under Building 2 was not disturbed from a depth of 6' below the finished floor and deeper. This covenant has effectively limited demolition of Building 2, and made only cosmetic renovations of the exterior and interior practical. It should be noted that the footprint of Building 2 places it in a user category similar to "big-box" distribution centers in other areas of southern California, but it has neither the height, the loading, nor other amenities specific to these types of facilities. This condition has precluded previous interested parties from purchasing the property, and has limited the economic options of the site.

Boyd Furniture had intended to create a home furnishings wholesale and retail center on the western portion of their property, using all of the existing buildings of the campus, and Building 2 for furniture manufacturing. The manufacturing uses were only marginally successful, and the retail uses less so. Most of the western retail buildings are currently vacant. Boyd sublet approximately 40% of the buildings, with the balance vacant and, in some cases, in disrepair. A self-storage facility was built on the south side of Second Street, north of the retail buildings, and is known as Building 3. This has been moderately successful.

I.D PROJECT HISTORY (continued)

The anticipated creation of jobs for this area of Pomona has failed to materialize, and the project sat with a high vacancy and minimal positive economic impact for the City of Pomona.

In 2004 the property west of Curran Place and north of Mission Boulevard, identified herein as the Project, was purchased by 1675 Mission Associates, LLC. They are the applicants of this Specific Plan. (C204-003).

I.E EXISTING ON-SITE LAND USES

Current uses in the Project include industrial warehousing, distribution and manufacturing and a personal self-storage facility. As noted above, there currently exist substantial vacancies, and the land and buildings are under-utilized and not generating the employment and tax benefits that the location would suggest the City could realize.

There are longer-term, multi-year leases for Building 7 and Building 28 by Tenants, which were in existence when the current Owner acquired the property and for Tenants taking up approximately two-thirds of Building 2. These longer-term leases, while providing some revenue stream, have restricted the flexibility the current Owner has in renovating the Project.

The Project currently has a total of 19 buildings and structures on it. Refer to Exhibit 1.4 on Page 1.13. On the following pages these buildings are inventoried, showing the number as originally used by General Dynamics and still in use, area, and a description.

I.E EXISTING ON-SITE LAND USES *(continued)*

EXISTING BUILDING INVENTORY BETWEEN MISSION BOULEVARD AND SECOND STREET:

Building:	Area:	Comments:
No. 1:	62,158 s.f.	Existing showroom, to be demolished.
No. 2:	751,257 s.f.	Existing offices, showroom, warehouse, to be renovated into multi-tenant
No. 2A:	3,744 s.f.	Existing utility building, to be demolished.
No. 2B:	3,617 s.f.	Existing utility building, to be demolished.
No. 3:	59,120 s.f.	Existing self-storage, to be sold as operating business.
No. 3A:	2,667 s.f.	Existing showroom, to be demolished.
No. 5:	22,200 s.f.	Existing warehouse, to be demolished.
No. 6:	7,338 s.f.	Existing showroom, to be demolished.
No. 7:	7,338 s.f.	Existing warehouse, to be renovated.
No. 28:	32,554 s.f.	Existing manufacturing and warehouse, to be renovated.
No. 35:	1,794 s.f.	Existing auxiliary building, to be demolished.
No. 37:	917 s.f.	Existing auxiliary building, to be demolished.
No. 38:	131 s.f.	Existing auxiliary building, to be demolished.
--	--	Guard Shack, to be demolished.
--	190 s.f.	Existing building by Guard Shack, to be demolished.
--	703 s.f.	Modular building at No. 7, to be demolished.

I.E EXISTING ON-SITE LAND USES *(continued)*

EXISTING BUILDING INVENTORY NORTH OF SECOND STREET:

Building:	Area:	Comments:
No. B:	1,373 s.f.	Existing SCE substation, (remains at discretion of SCE).
--	unknown	Existing water pump station, to be demolished.
--	unknown	Existing water storage tank, to be demolished.

EXISTING BUILDING INVENTORY ON POMONA UNIFIED SCHOOL DISTRICT PROPERTY:

Building:	Area:	Comments:
--	unknown	Total of 6 lightweight buildings used for storage and maintenance purposes. Planned use of the buildings by Pomona Unified School District is not known and is subject to school district policy. Once this area is developed, it will be subject to City requirements and this Specific Plan.

I.E EXISTING ON-SITE LAND USES *(continued)*

DISCUSSION OF REMAINING BUILDING EXTERIOR CONSTRUCTION AND APPEARANCE:

<u>Building:</u>	<u>Area:</u>	<u>Comments:</u>
No. 2:	751,257 s.f.	This building is of a steel-frame with exterior concrete non-structural panels. There is an older applied architectural treatment at the southeast corner of the building that is not esthetically balanced. On the west elevation, near Second Street, and the north elevation, near the west elevation, there is an applied dock with a concrete base and corrugated steel exterior skin. Existing offices, showroom, and warehouse, to be renovated into multi-tenant
No. 3:	59,120 s.f.	The self-storage building was built of steel frame and exterior plaster (stucco) finish, and has several architectural treatments that matched the retail furniture outlet.
No. 7:	7,338 s.f.	This building is of steel frame and exterior concrete non-structural panels.
No. 28:	32,554 s.f.	This building is of cmu (concrete masonry units, also known as concrete block) and fabricated steel siding, with a sloping steel roof.
No. B:	1,373 s.f.	The SCE substation is of a poured-in-place concrete design.

EXISTING LAND USE AND BUILDINGS

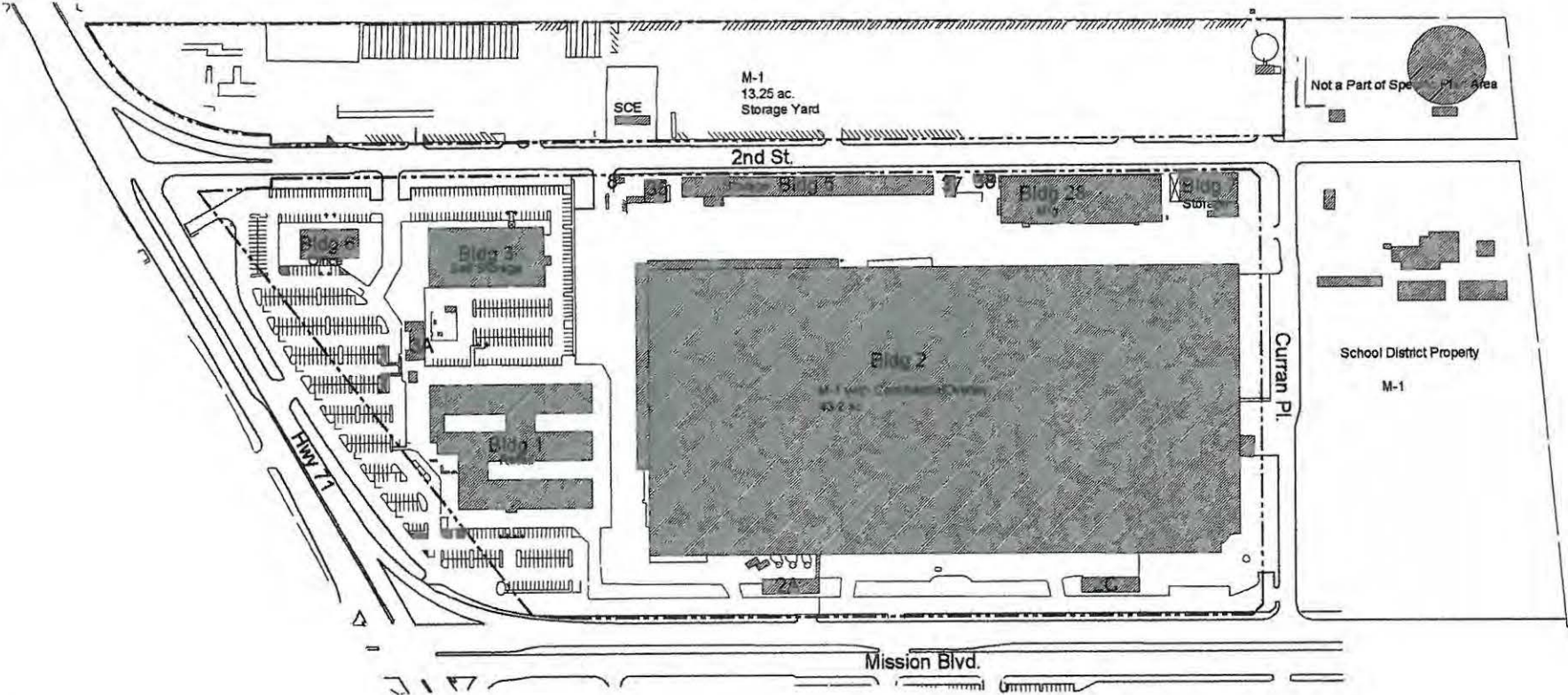


Exhibit I-D

1.F SURROUNDING LAND USES

To the east of Curran Place exists a parking area and several former support structures for the General Dynamics campus, owned by the Pomona Unified School District. Further east is an Adult Education Campus owned by the Pomona Unified School District. Then, beyond the Adult Education Campus exist a variety of light industrial, commercial and residential uses.

North of the Project are the previously-mentioned Union Pacific tracks, a heavy industrial use, a Union Pacific mainline (the former Southern Pacific mainline) that converges with the first set of tracks east of the Project, and then more older heavy industrial manufacturing uses.

To the west of the Project and state route 71, on the west portion of the old General Dynamics campus, are a variety of industrial uses.

Southwest of the Project (south of Mission Boulevard and west of the 71 Corona Expressway) is a single-family residential neighborhood. This area will be physically and visually blocked from the project when the new overpass of Mission Boulevard over the 71 Corona Expressway is built, estimated to begin in late 2006.

South of Mission Boulevard is a strip of low-end commercial properties, with a home construction outlet, some shops and fast-food restaurants, and a Cal Trans storage yard.

I.G CURRENT ZONING & GENERAL PLAN

The Project is currently zoned and Master-Planned M1 by the City of Pomona. The General Plan designation for the site is Industrial. On the western portion of the project, where the retail uses were, a Conditional Use Permit was obtained for M1-S-C1 zoning.

Approval of this Specific Plan will result in the overall Project being zoned M1, with the goal of developing the entire site for Industrial use. This will make the entire site consistent with the General Plan.

CURRENT ZONING AND GENERAL PLAN

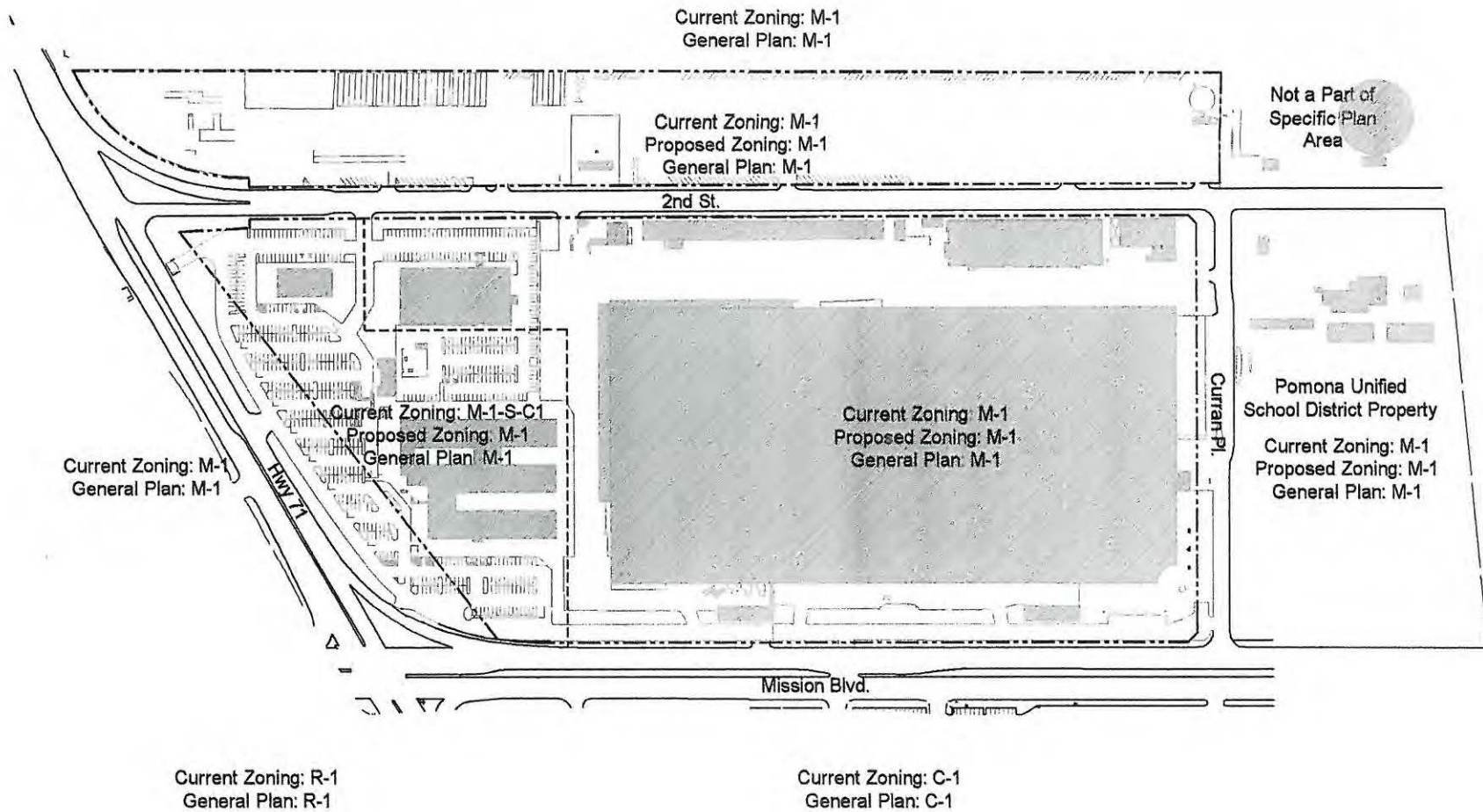


Exhibit I-E

SECTION TWO
PROJECT DEVELOPMENT

II.A PROJECT DEVELOPMENT GUIDELINES PURPOSE

The purpose of the Mission – 71 Business Park Specific Plan is designed to provide a roadmap for a revitalized industrial and business development Project for this significant gateway into the City of Pomona. The Specific Plan will provide aesthetic design guidelines and development standards, including parking, loading and circulation standards. These standards and guidelines will be used for redeveloping the existing buildings that will remain and for future development on the site such that a cohesive, integrated business park atmosphere is created. The overriding criteria shall be an integrated image of the Project. Each parcel development shall add to the unified corporate image of the Project, while capturing the individual personality of each building through complimentary but unique architecture and landscaping. It will provide the City of Pomona with control over the development of this strategic gateway into the City, and allow for a new economic vitality, contributing to the overall strength and well-being of the community.

II.B PROJECT DEVELOPMENT GUIDELINES GOALS

The goals of the Development Guidelines for the Specific Plan shall:

- Create a unified industrial anchor to the prime western entry into the City of Pomona through architectural and landscaping guidelines. These guidelines will reflect the industrial character and history of the Project through use of durable, attractive and colorful materials; provide for a high-quality building esthetic; and achieve a harmony in the overall Project presentation. The design guidelines are not intended to limit design possibilities but rather to encourage architectural design excellence while maintaining a theme and quality throughout the entire Project.
- Ensure an efficient and effective circulation system that works within the limitations created by the future 71 Corona Expressway access restrictions. The circulation system shall provide sufficient width, turning radii, and visibility for trucks and automobiles, and shall be supportive of the industries that currently are located in the Project as well as those targeted by the Specific Plan.
- Provide special parking standards consistent with existing parking standards for such uses in surrounding municipalities. These should reflect the potential for distribution opportunities that have become the backbone of Southern California's leadership in the nation's logistics network. These tiered standards, based on review of the parking standards of nearby cities that have enjoyed tremendous development in recent years by attracting large regional, national and multi-national logistics facilities, should be applied (as appropriate) to encourage such quality development in the City of Pomona.
- Provide a blueprint for future developers to follow to allow for an expedited entitlement process through the City of Pomona. Those developments that comply with the intent of the Specific Plan guidelines will be facilitated in their approval process by City staff. Create an orderly method for approval of the exterior design elements of the renovated buildings of the Project. This should create an orderly method for approval of the site planning, circulation, and exterior design of future buildings to be constructed by either users or developers who purchase the land sold by the current Owner.

II.C PROJECT DEVELOPMENT GUIDELINES OBJECTIVES

- The objectives of the Development Guidelines for the Specific Plan should:
- Preserve visibility of green spaces, avoid congestion and unattractive building density, and prohibit on-street parking.
- Guarantee the highest value of lots, sites and buildings in the Project.
- Ensure each lot and structure is appealing and does not obstruct, harm, or otherwise discourage existing or potential adjoining Owners.
- Comply with all applicable federal, state and local laws, rules, ordinances, and other government requirements.
- Uphold an aesthetically pleasing, balanced and compatible place of commerce, for Owners, their Tenants, employees and invitees.
- Safeguard the Project from fire, explosion, toxic and noxious matter and other hazards, offensive noise, vibration, smoke, dust, odorous matter, nuisances and other objectionable influences.
- Maintain developed and undeveloped portions of the Project Area per specific City requirements, necessary cross access & maintenance easements.
- Assure necessary site development requirements and covenants.
- Protect against buildings and improvements of poor construction, design or quality, and usage of attractive and compatible architectural, engineering, planning and construction standards and materials of good quality and pleasing appearance appropriate to the uses of the Project Area.

PROJECT DEVELOPMENT GUIDELINES OBJECTIVES

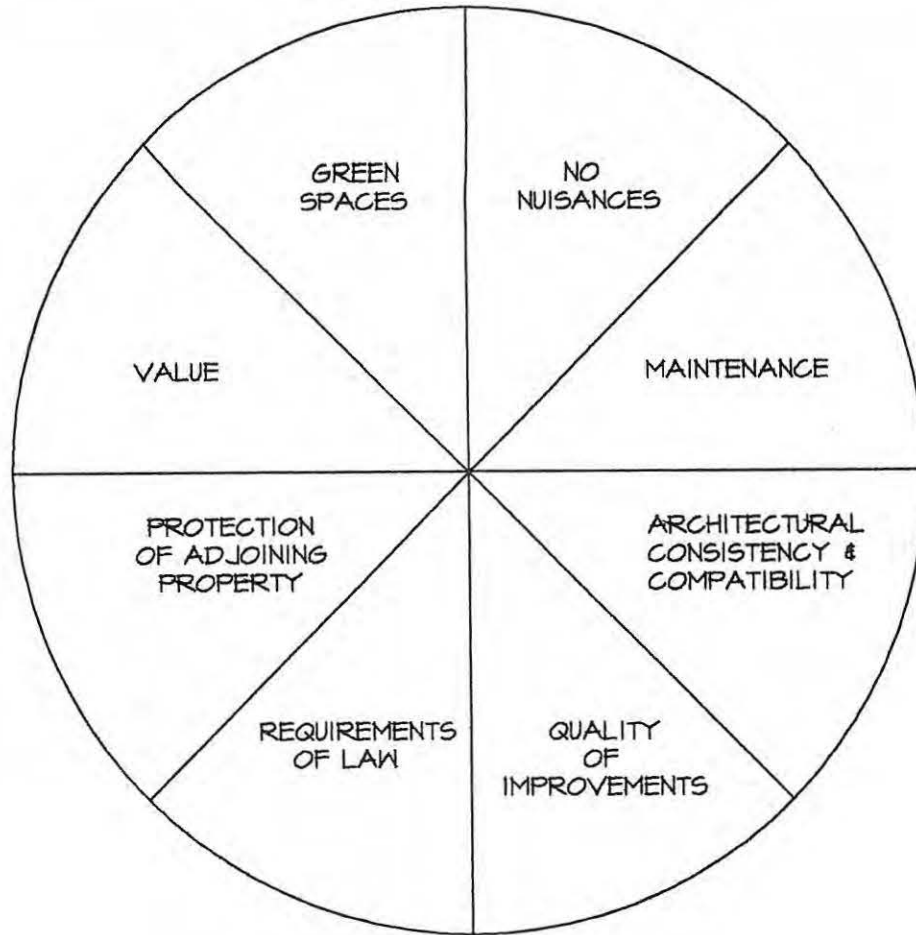


Exhibit 2-A

II.D PROJECT DEVELOPMENT STRATEGIC PLAN

The Owner of the Project has developed a strategic plan for revitalizing this significant property in the City of Pomona. The Specific Plan has been created in response to this as well as integrating the City's desire to ensure the Project's long-term success by creating Design Guidelines to manage this process. The following describes the Owner's proposal:

- Demolish the buildings and structures as noted in the section on Existing On-site Land Uses above.
- Renovate Buildings 2, 7 and 28 to create a new design theme to be followed by the overall Project. The following is to be done with each building:
 - a. Building 2: Keep ownership of and renovate this building and lease out as warehouse space. Internally renovate, including demolition and removal of unused leftover elements from the previous uses, and provide adequate office and restroom support for intended industrial users. Externally renovate, providing the architectural theme for the overall project.
 - b. Building 7: Keep ownership of and continue the on-going lease of this building as existing light manufacturing. Externally renovate.
 - c. Building 28: Keep ownership of and continue the on-going lease of this building as existing light manufacturing. Externally renovate.
- Divide the property south of Second Street into a parcel containing Buildings No. 2, No. 7 and No. 28, a parcel containing the self-storage Building No. 3, and then multiple parcels on the remaining land. The Owner will retain ownership of the parcel containing Buildings No. 2, 7 and No. 28. The self-storage Building No. 3 parcel and the remaining parcels are to be sold to either a developer(s) or user(s) for future development.

II.D PROJECT DEVELOPMENT STRATEGIC PLAN (continued)

- Divide the property north of Second Street into multiple parcels, and sell these to either a developer(s) or user(s) for future industrial development.
- Reconfigure the parking, loading and landscaping on the parcel containing Buildings No. 2, No. 7 and No. 28 to support new warehouse/distribution uses, while screening loading areas as much as possible from Mission Boulevard.
- Create the Mission – 71 Business Park Specific Plan for the planning area establishing development standards for contemporary and future construction. The Specific Plan will graphically show several key conceptual elements of the plan consisting of:
 - a. Widening Curran Place between Mission Boulevard and Second Street, converting it to a four-lane public street.
 - b. Designate the Pomona Unified School District property with the same zoning as the subject property for future development, integrating it with the Project owned by the Owner for design and circulation purposes.
- Create concurrence from the City and other governing agencies (such as Los Angeles County Fire Department) to expedite the program.

II.E PROJECT DEVELOPMENT ACTIONS

Listed below are the major actions that will be necessary for a successful implementation of the Mission – 71 Business Park Specific Plan. These reflect the goals of the Owner and the City. A number of these, as noted, will not be undertaken by the Owner as they fall within the jurisdiction of other parties. These include Caltrans, the Pomona Unified School District, and the City of Pomona. Actions 1 and 2 of the listed activities below are included for informational purposes only. They are not part of the Specific Plan application as they are stand-alone actions designed to accommodate Owner’s needs to maintain project financial viability during the period in which the Specific Plan is developed and processed. The steps in the process of achieving the Owner’s program are broadly defined as follows:

1. Conditional Use Permit: major components.

- a. The Owner will obtain a Conditional Use Permit (CUP) from the City. It shall establish and grant the interim proposed use of the existing Buildings 2, 3, 7, and 28 while the preparation and application for the eventual approval of the Specific Plan for the Project is processed.
- b. Utilization of the existing and new on-site parking for all of the stated uses under current parking requirements.
- c. Show demolition of the buildings previously listed.
- d. Renovation of Building 2.
- e. Provide submittal information defining the architectural design and site construction necessary to meet submittal requirements and obtain conditional use approval.

II.E PROJECT DEVELOPMENT ACTIONS *(continued)***2. Create Construction Documents:**

- a. Drawings and Specifications for exterior improvements.
- b. Assess fire protection and seek acceptance from LA County Fire for the fire protection program, including sprinklers, smoke ventilation, fire access, and allowable commodity classifications and storage heights, and develop a fire protection specification.
- c. Grading plan for site improvements.
- d. Landscape plans for site improvements.

3. Create the Specific Plan:

- a. Reiterate the uses as defined in the CUP.
- b. Define the new planning area to include the Pomona Unified School District property.
- c. Define the specific architectural, landscape, hardscape, and lighting standards for the Project development area.

4. Create a Tract Map:

- a. The Tract Map (TM) shall define the subdivision of all property under the Owner's ownership and allow for the sale of parcels to parties desiring to develop them.
- b. Establish reciprocal access easements connecting parcels to the public way, as necessary.

II.E PROJECT DEVELOPMENT ACTIONS *(continued)*

5. Related Actions regarding property not owned by Applicant.

- a. Acquire right to widen Curran Place over Pomona Unified School District Property.

6. Preparation of Environmental Assessment:

- a. The City of Pomona has obtained an independent consultant to prepare the document, which has been completed.

7. Overlap of preparation and processing of individual entitlement components:

- a. Necessary to reduce costs, make the project economically viable, and provide benefit to the City of Pomona.

II.F DESIGN REVIEW PROCESS

The design review process will utilize the Site Development Review currently managed by the City of Pomona Planning Division and involves the Planning staff, a submitting developer (the Applicant), and their consulting team of design professionals including the architect, the landscape architect, engineers and other professionals deemed necessary by the Applicant. The following will govern this process:

Procedure - The purpose of this Section is:

1. To establish procedures for reviewing development applications (e.g., proposed construction of new or modified projects) in a timely and efficient manner to ensure that the projects comply with all applicable development standards and design guidelines and minimize potential adverse effects on surrounding properties and the environment.
2. Encourage Creativity - This Section is not intended to restrict imagination, innovation, or variety, but rather to focus on development and design standards/guidelines that can result in creative, imaginative solutions and a quality design for the City. It is, therefore, the purpose of this Section to accomplish the following:
 - a. Recognize the interdependence of land values and aesthetics and encourage the orderly and harmonious appearance of development within the City; and
 - b. Assist developers in understanding the public's concerns for the aesthetics of development and ensure that new developments do not cause an adverse health, safety, or aesthetic related impact(s) on the community.

Responsibility for Site Development Review

1. Major Site Development Review – Planning Commission - The Planning Commission shall approve, approve with conditions, or disapprove Major Site Development Review permit applications, in compliance with Exhibit 2-B, *Thresholds for Site Development Review*. The Commission may impose conditions deemed reasonable and necessary to ensure compatibility with surrounding uses; to preserve the public health, safety, and welfare; and to make the findings required by the Section that follows on Findings and Decision.

II.F DESIGN REVIEW PROCESS (continued)

2. Minor Site Development Review – Community Development Director

- a. The Community Development Director shall approve, approve with conditions, or disapprove Minor Site Development Review permit applications, in compliance with Exhibit 2-B, *Thresholds for Site Development Review*. The Community Development Director may impose conditions deemed reasonable and necessary to ensure compatibility with surrounding uses; to preserve the public health, safety, and welfare; and to make the findings required by the Section that follows on Findings and Decision.
- b. The Community Development Director may, at his/her discretion, defer action on a Minor Site Development Review application and refer the application to the Commission for review and decision at a scheduled public hearing.

Applicability

An application for Site Development Review is required for all projects that involve new construction or additions to existing structure in compliance with Exhibit 2-B, *Thresholds for Site Development Review*.

Application Filing, Processing and Review.

1. Filing - An application for a Site Development Review shall be filed with the Planning Division in compliance with City requirements.
2. Contents - The application shall be accompanied by the information identified in the Planning Division handout for Site Development Review applications.
3. Project Review Procedures - Following receipt of a completed application, the Planning Division staff shall make an investigation of the facts bearing on the case to provide the information necessary for action consistent with the intent of this Section.

II.F DESIGN REVIEW PROCESS (continued)

Factors to be Considered

1. The review authority shall consider the location, design, site plan configuration, and overall effect of the proposed project upon surrounding properties and the City in general.
2. Review shall be conducted by comparing the proposed project to applicable development standards, design guidelines, and other applicable City ordinances.

Notice and Hearings

1. Scheduling of Hearing - An application for a Major Site Development Review shall be scheduled for a public hearing once the Planning Division has determined the application is complete.
2. Noticing of Hearing - Noticing of the public hearing shall be given in compliance with City requirements.
3. Hearing for Minor Site Development Review Not Required - An application for a Minor Site Development Review shall not require a public hearing.

Findings and Decision

A Site Development Review application shall be approved, with or without conditions, if the following findings can be made in a positive manner:

1. The design and layout of the proposed development are consistent with the development and design standards/ guidelines of the applicable zoning district;
2. The design and layout of the proposed development would not interfere with the use and enjoyment of neighboring existing or future developments, and would not create traffic or pedestrian hazards;

II.F DESIGN REVIEW PROCESS *(continued)*

3. The design of the proposed development would maintain and enhance the attractive, harmonious, and orderly development contemplated by this Section;
4. The design of the proposed development would provide a desirable environment for its occupants and visiting public as well as its neighbors through good aesthetic use of materials, texture, and color and would remain aesthetically appealing and retain an appropriate level of maintenance; and
5. The proposed development would not be detrimental to the public health, safety, or welfare or materially injurious to the properties or improvements in the vicinity.

Conditions of Approval

In approving a Site Development Review, the review authority may impose conditions (e.g., buffers, landscaping and maintenance, signs, lighting, off-site improvements, performance guarantees, surfacing, property maintenance, parking, traffic circulation, etc.) deemed reasonable and necessary to ensure that the approval would be in compliance with the findings required by the Section above on Findings and Decision.

Post Approval Procedures

Procedures relating to appeals, changes, issuance of a Building Permit, performance guarantee, and revocation as identified in the City's Zoning Code shall apply following the approval of a Site Development Review.

THRESHOLDS FOR SITE DEVELOPMENT REVIEW

Threshold Categories	Decided by Director of Community Development	Decided by Planning Commission
New Construction ▪ Non Residential: Up to 20,000 square feet of total gross floor area Modifications to Existing Construction ▪ Additions which do not exceed 50% of the existing floor area.	Minor Site Development Permit	
New Construction ▪ Non Residential: More than 20,000 square feet of total gross floor area Modifications to Existing Construction ▪ Additions which exceed 50% of the existing floor area for nonresidential developments		Major Site Development Permit
Other For projects that do not meet the specific criteria identified above	At the discretion of the Community Development Director	

Exhibit 2-B

SECTION THREE

PROJECT USES AND PHASING

III.A PROPOSED LAND USES

The Mission – 71 Business Park Specific Plan proposed that all uses in the Project be industrial, including warehousing, distribution and manufacturing, plus the existing self-storage facility. The purpose of this is to provide a unified, cohesive development with its own identity, providing the City of Pomona with a tax and employment base.

The project is anchored by Building 2, an existing +/- 751,257 s.f. low-clearance building primarily used for distribution. Surrounding land is proposed to be parceled to provide complimentary smaller buildings for use as either distribution or manufacturing. All building are proposed to have ground-level and potentially dock-high truck loading with generous truck maneuvering areas to facilitate industrial users.

Refer to Exhibit 1.4, page 1.13 for existing land uses and buildings.

Future development of parcels will be used solely for the purposes intended and granted by the City of Pomona, in accordance with the Pomona zoning plan and this Specific Plan.

III.A PROPOSED LAND USES (continued)

PERMITTED USES:

The uses listed herein are permitted uses in this Specific Plan. All other uses must be approved by the City of Pomona Planning Department. Should doubt arise concerning site usage, the purchaser/lot Owner, assisted by qualified technical persons and equipment, must demonstrate that protective devices will control and/or eliminate any offensive factors, in compliance with City of Pomona Guidelines and Regulations.

- Bakery
- Banks
- Blueprinting, printing and photocopying
- Cabinet, woodworking or carpentry shop
- Carpet and rug cleaning plants
- Ceramic products
- Distributing and bottling plants
- Fabricating and assembly (rubber, light metal, electronic)
- Laboratory and research (experimental, motion picture, testing)
- Machine shop (indoors only)
- Machinery and shop (indoors only)
- Manufacturing and maintenance of electric or neon signs
- Manufacturing, compounding, processing, packaging or treatment
- Motion Picture Facilities
- Manufacturing (novelties, refrigerated food locker, rubber and metal stamps, stone monument, textiles, toys)
- Offices
- Manufacture of outdoor advertising devices
- Public utility transmission substation, subject to City approval of a Conditional Use Permit
- Cellular antenna, satellite or other comparable uses pursuant to City Regulations
- Restaurant
- Self-storage (indoors only)
- Sheet metal shop
- Wholesaling and warehouse, indicating distribution facilities

PROPOSED LAND USE PLAN

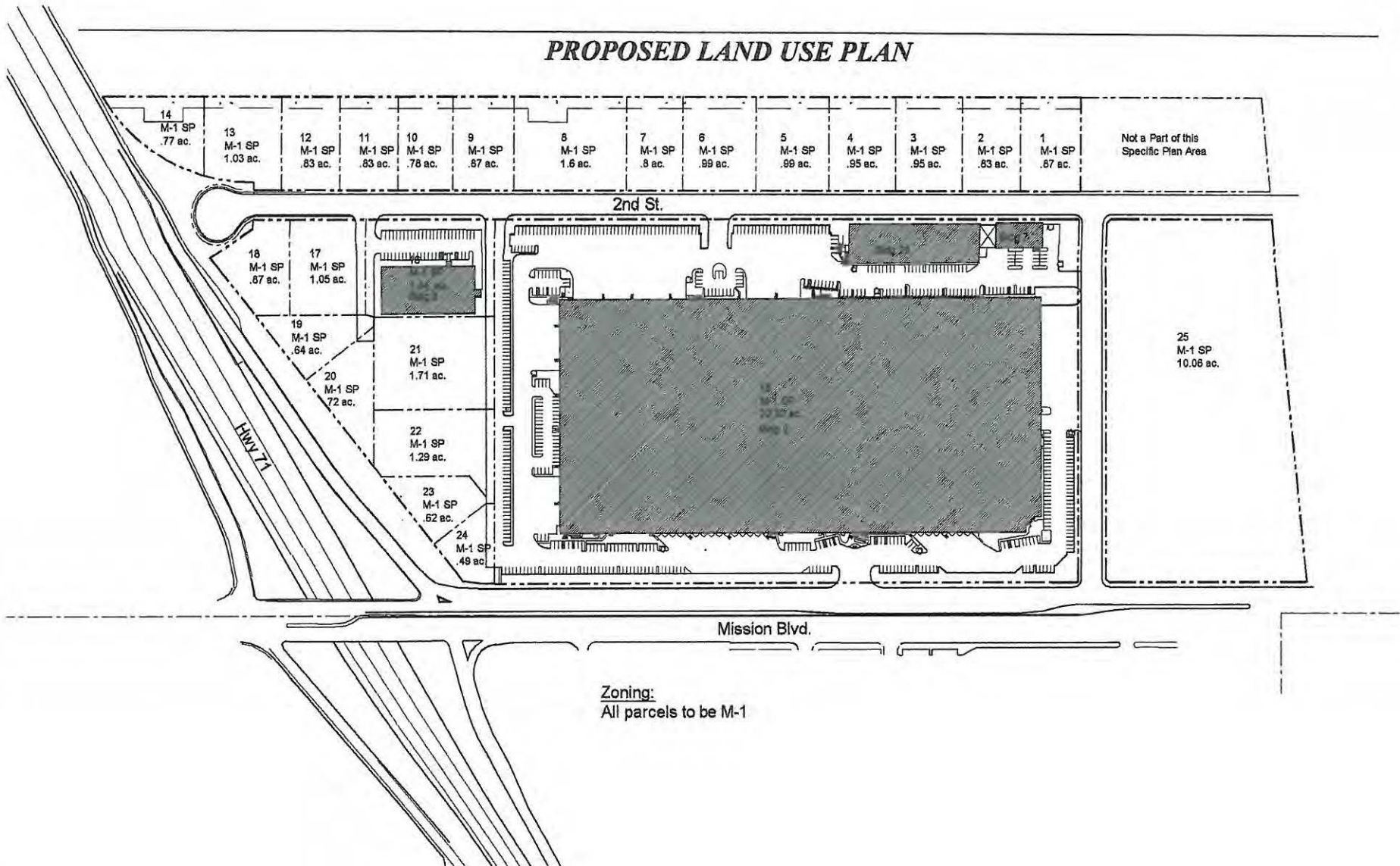


Exhibit 3-A

III.B PROJECT PHASING

Project development has been defined over 5 phases. These are not necessarily chronological but rather define different aspects of the development, and include tasks that are under the control or jurisdiction of other agencies and entities, such as Caltrans, the Pomona Unified School District, and the City of Pomona. For Phasing Plans, refer to Exhibits 3.2 – 3.7, pages 3.7 – 3.12. For an aerial view showing the Project with all phases, refer to Exhibit 3.8 on page 3.13. The phases are defined as follows:

PHASE I

This phase is comprised mainly of the Specific Plan Area 1. It includes the improvements to Building No. 2, demolition of specific structures, clean up of the site, new parking layout and paved area improvements, and new landscaping, parking area lighting, and fire protection service improvements. This phase also includes the future Lot 16 and the existing Building No. 3 on it, which is to remain as a self-storage facility.

Improvements to Building No. 2 will consist of the addition of new architectural entry elements to the Tenant spaces. The overall architectural theme will extend aspects of the look already in place on the south-east corner of the building formerly know as the Boyd space to the rest of the building. This space was originally office and showroom use with a large amount of glazing that is not appropriate for general warehouse use. The other detailing aspects of trim and cornice, and wall break-up treatment will be used to extend the look.

The existing punch windows across the upper portion of the walls will be evoked in new areas with the use of window fenestration and spandrel glazing.

The existing main entry of Building No. 2 will be modified with accent elements. Architectural mass elements of similar construction will be placed at other appropriate locations to further break up the long walls and add interest to the elevations.

III.B PROJECT PHASING (continued)

PHASE I (continued)

Specific Plan Area 3 minus the demolished Buildings No. 1 and No. 6 shall remain as-is for additional parking during the construction of site improvements in Specific Plan Area 1, and until the special parking standards are approved in the Specific Plan.

Street Improvements shall consist of upgrading Curran Place to a four-lane street; upgrading the interchange of Mission Boulevard and Curran Place for full flow, including a left-turn lane from eastbound Mission Boulevard onto northbound Curran Place; and preparing this intersection for future signalization when traffic needs warrant. The applicant shall be responsible for construction of these upgrades, including acquisition of the necessary right-of-way, from the Pomona Unified School District.

PHASE II

This phase shall consist of the preparation of Specific Plan Area 2 for sale to developers. The land shall be cleared of all rubbish, and readied for sale.

A Tract Map shall be processed defining the lots to be offered for sale. After purchase, developers will submit plans for improvements on the various lots following the guidelines of this Specific Plan. These developers shall be responsible for creating new access drives from Second Street to their property.

III.B PROJECT PHASING (continued)

PHASE III

This phase consists of the Specific Plan Area 3. All existing improvements shall be removed. The access to the tunnel under Highway 71 to the western portion of the old General Dynamics campus shall be closed. It is anticipated that the remainder of the tunnel will be demolished as part of the future Highway 71 improvements. The land in this Specific Plan Area shall be returned to “for sale” condition. The applicant shall construct the entry drive from Second Street and record mutual driveway easements as part of this phase.

A tract map shall be processed defining the lots to be offered for sale. After purchase, the developers will submit plans for improvements on the various lots following the guidelines of this Specific Plan.

PHASE IV

This phase consists of Specific Plan Area 4. This land is the property of Pomona Unified School District. It is expected that the property will be sold to future developers. In this phase the following events and tasks are anticipated:

1. Clear the parcel of all improvements and create a “for sale” condition.
2. Prepare and file appropriate tract map and development documents.

The improvement of these lots will be processed under the guidelines of this Specific Plan. At this time it is expected that the City of Pomona will lead the implementation of items 1 through 4 above.

III.B PROJECT PHASING (continued)

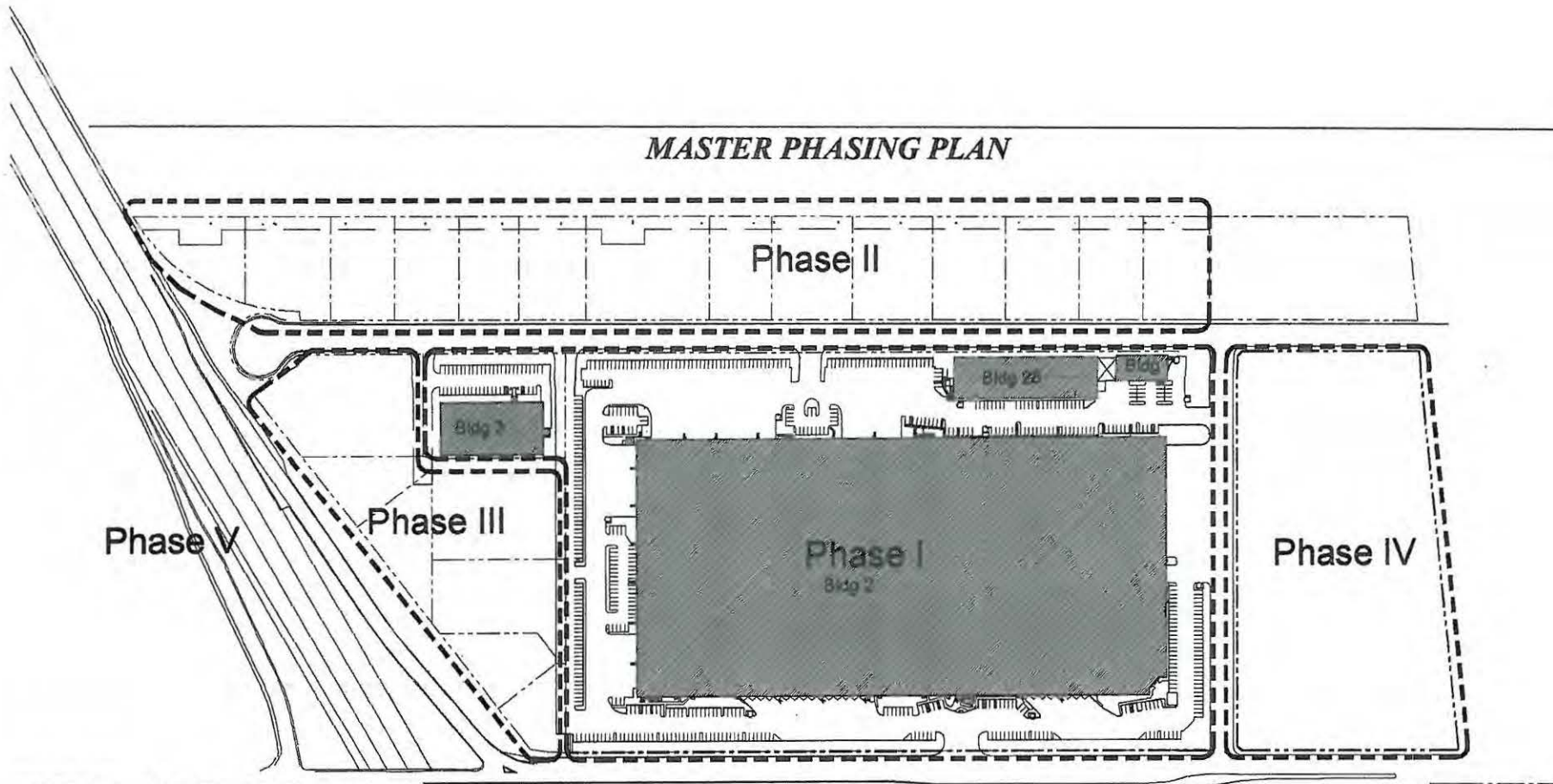
PHASE V

This phase consists entirely of off-site improvements. Caltrans will be converting Highway 71 to a freeway at sometime in the future. The impact to the area of this specific plan is primarily related to the following items:

1. Construction of a new bridge on Mission Boulevard over existing Highway 71 by the City of Pomona.
2. Expand existing highway width to design currently on file with Caltrans.
3. Construct on- and off-ramps from Mission Boulevard to Highway 71. These will include an on-ramp from westbound Mission Boulevard to northbound Highway 71 that will be located primarily on the parcel currently owned by the City of Pomona directly adjacent to the west side of Specific Plan Area 3.
4. At such time as the ramps from Mission Boulevard to Highway 71 are constructed, access to Highway 71 from Second Street is planned to be terminated. Second Street is proposed to be terminated in a cul-de-sac immediately east of the existing Highway 71 right-of-way by Caltrans. It has been suggested that Second Street could then become a private street from the cul-de-sac to Curran Place.

It is anticipated that Caltrans and/or the City of Pomona will be responsible for financing all of the above items in this phase.

MASTER PHASING PLAN



Phase I:
Cosmetic, parking and functional improvements to Building 2, demolition of selected buildings, widening of Curran Place

Phase II:
North parcel lots sold for development.

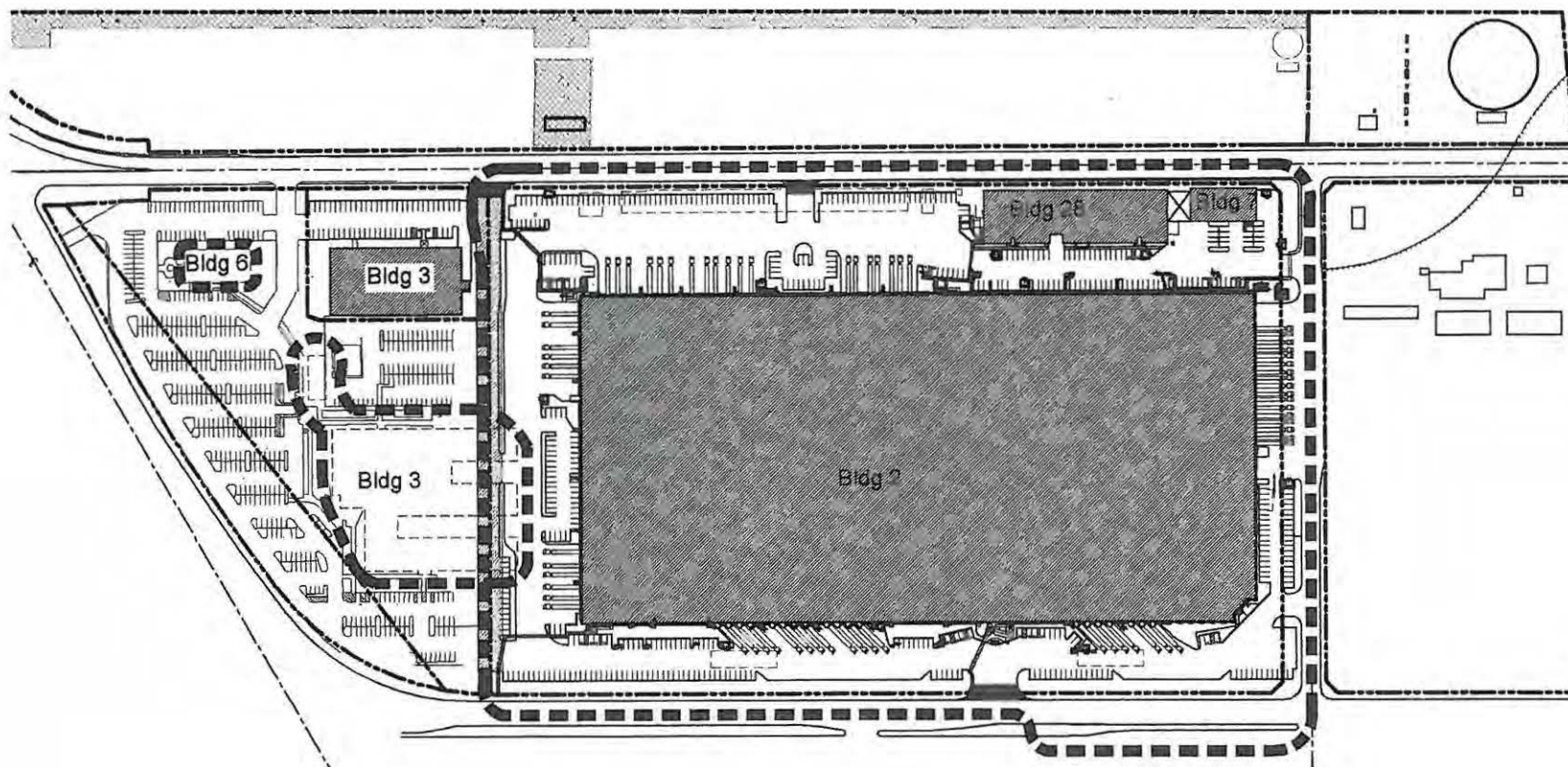
Phase III:
West parcel lots sold for development. Tunnel under 71 closed and access demolished.

Phase IV:
School District property available for development.

Phase V:
Mission Blvd. overpass constructed, Hwy 71 improvements complete, Second St. dead ends in cul de sac.

Exhibit 3-B

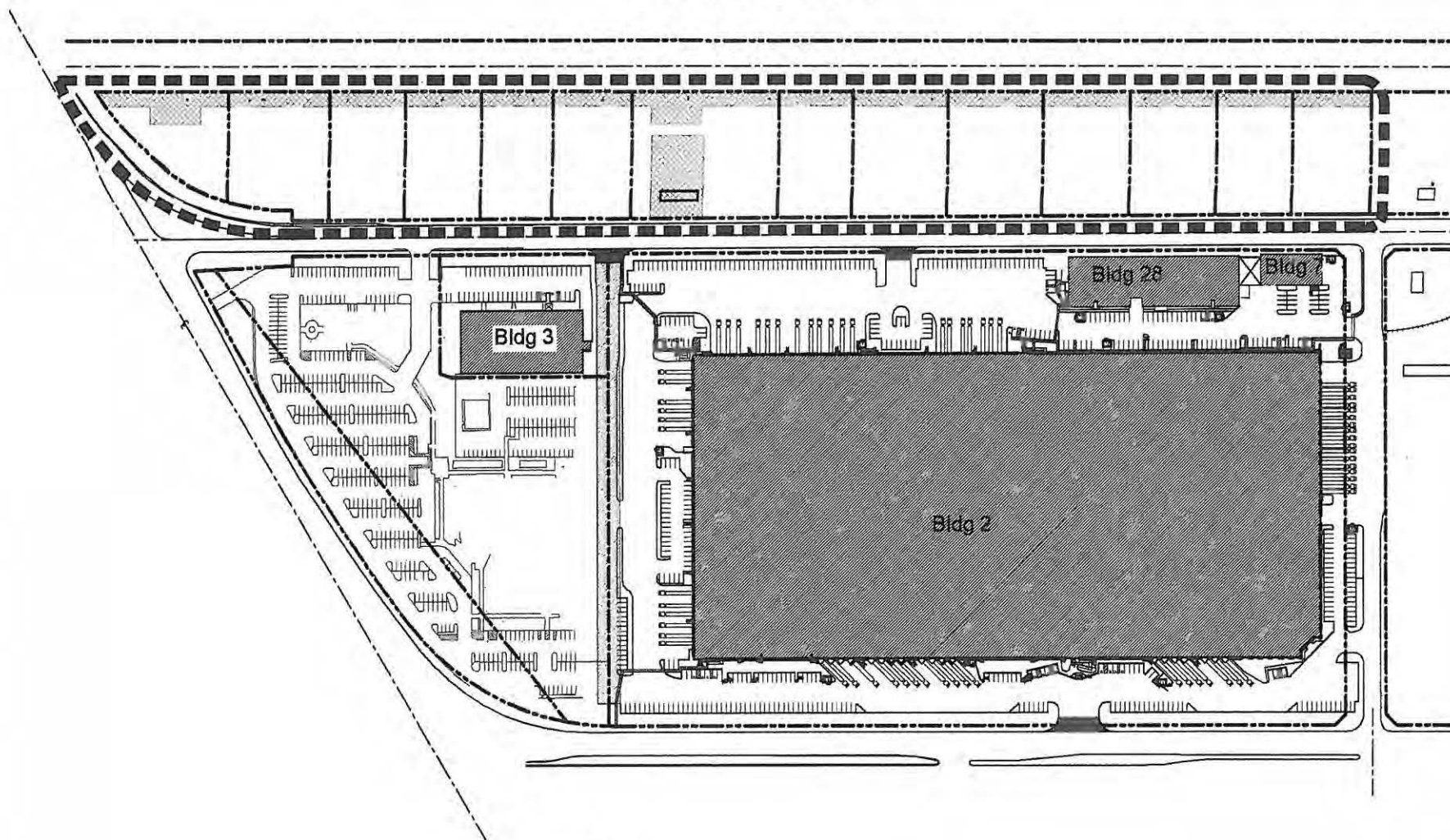
PHASE I PLAN



Phase I:
Cosmetic, parking and functional improvements to
Building 2, demolition of selected buildings, widening
of Curran Place

Exhibit 3-C

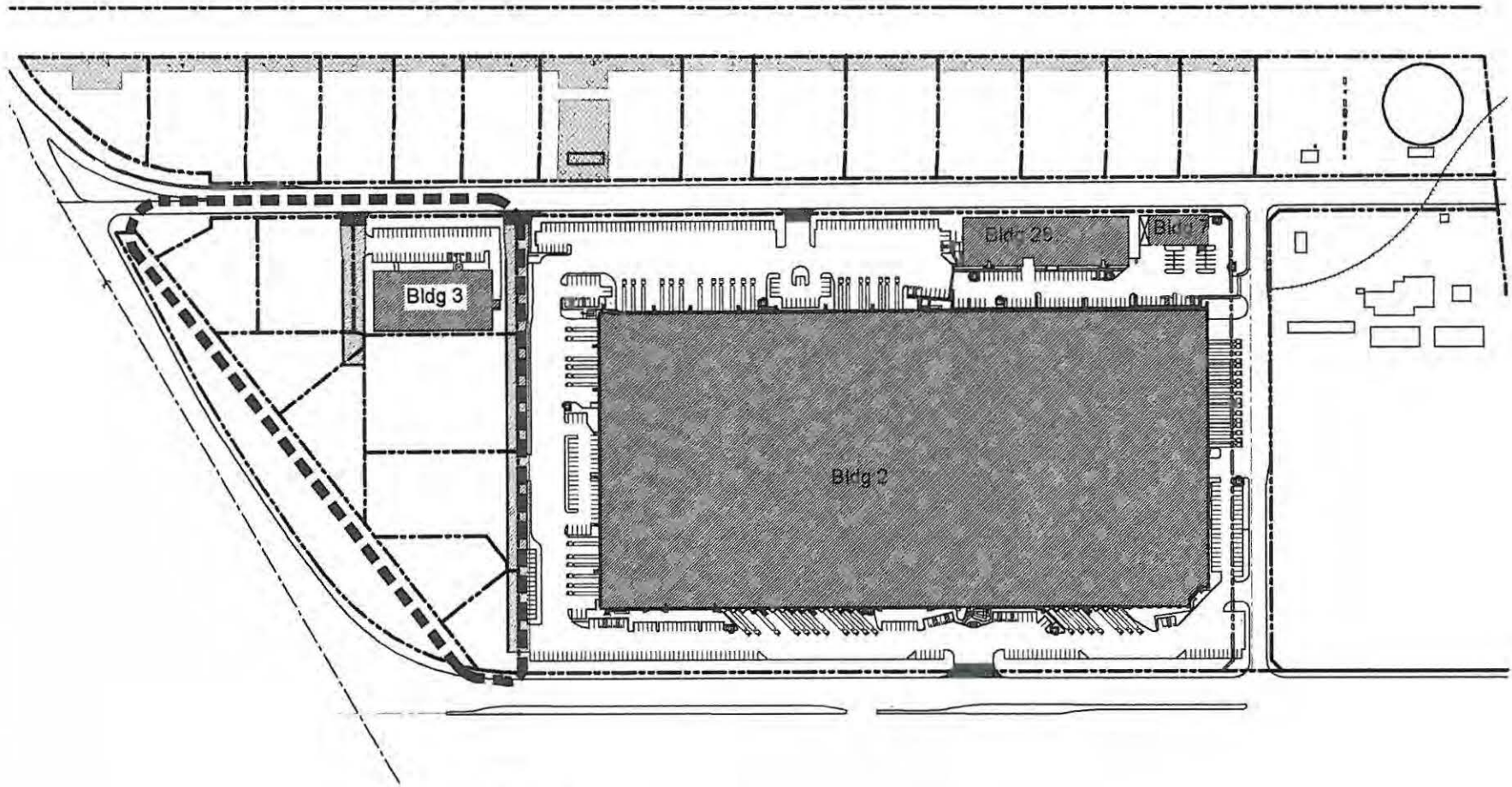
PHASE II PLAN



Phase II:
North parcel lots sold for development.

Exhibit 3-D

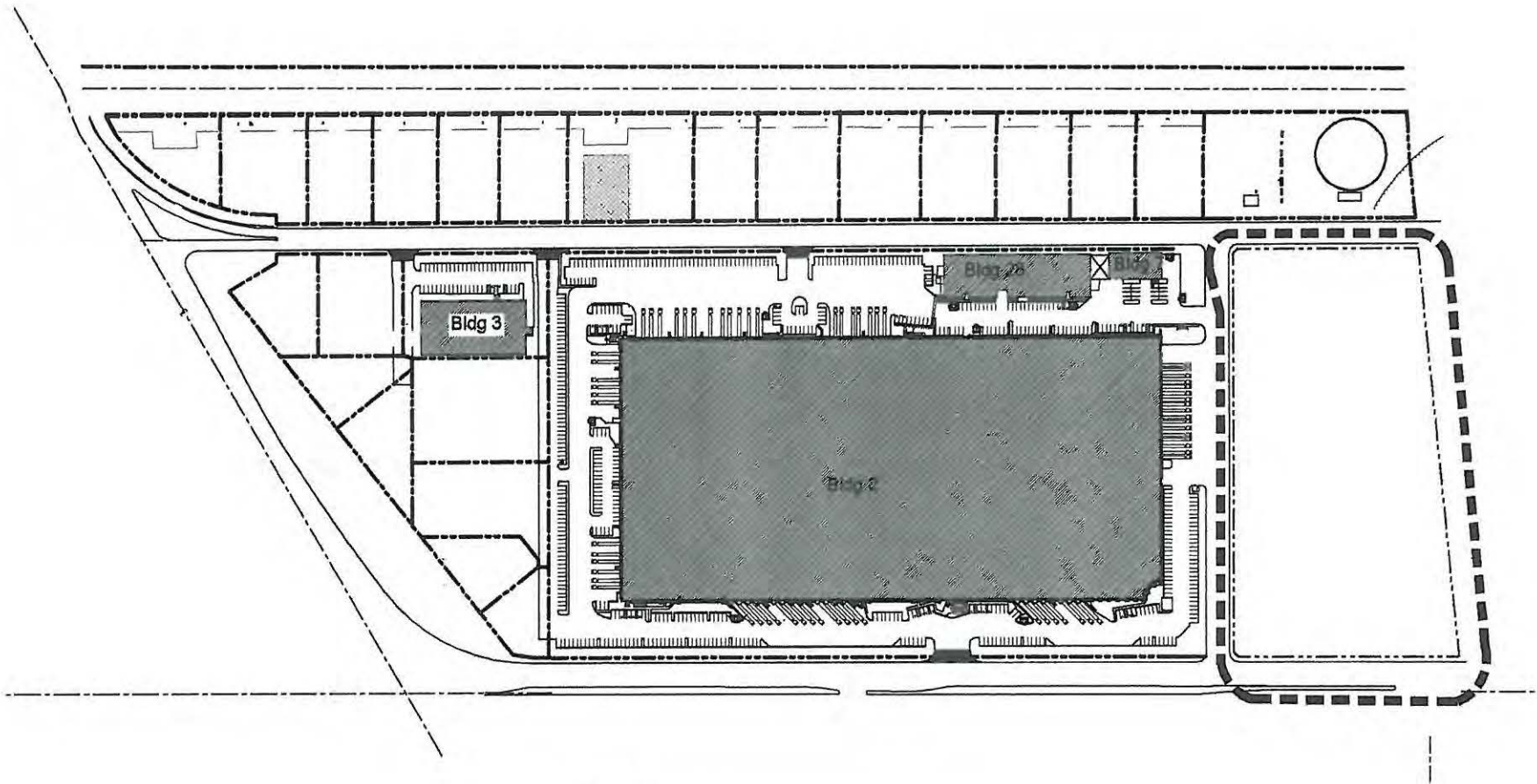
PHASE III PLAN



Phase III:
West parcel lots sold for development. Tunnel under 71
closed and access demolished.

Exhibit 3-E

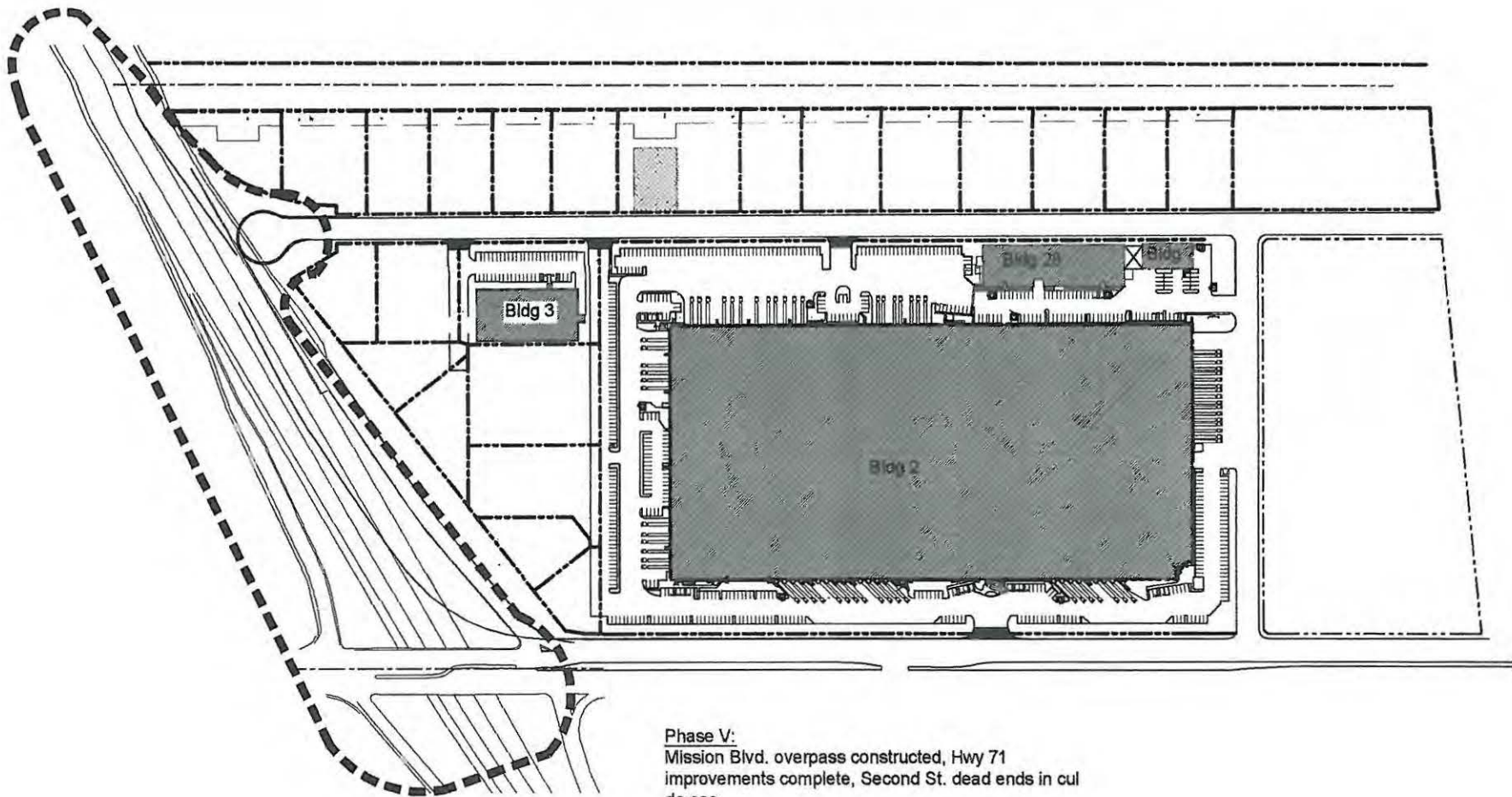
PHASE IV PLAN



Phase IV:
School District property available for development.

Exhibit 3-F

PHASE V PLAN



Phase V:
Mission Blvd. overpass constructed, Hwy 71
improvements complete, Second St. dead ends in cul
de sac.

Exhibit 3-G

AERIAL VIEW AT FINAL PROJECT PHASING

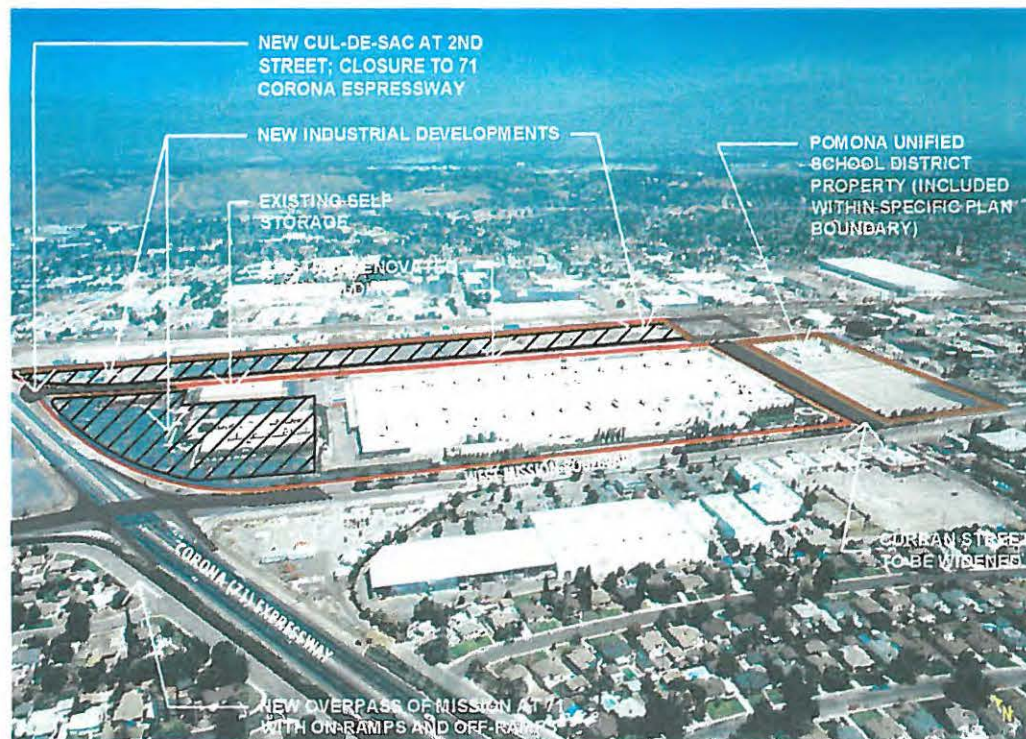


Exhibit 3-H

CONCEPTUAL BUILDING LAYOUT

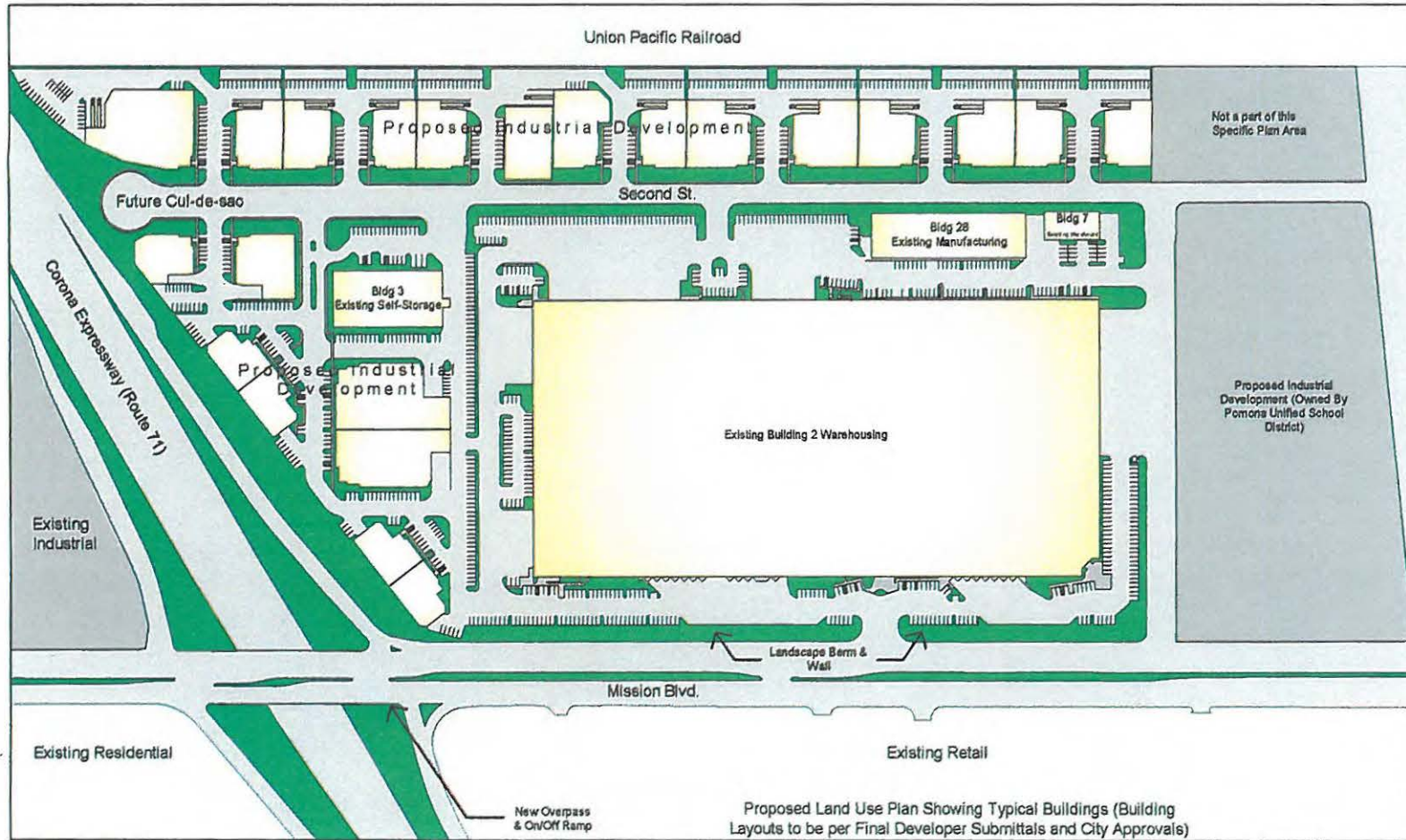


Exhibit 3-I

SECTION FOUR
ARCHITECTURAL STANDARDS

IV.A ARCHITECTURAL THEME

Primarily, the structures will be concrete tilt-up or precision block masonry construction. The design shall incorporate reveals and surface depressions and protrusions in order to break up the large plane wall areas that are particular to this building type. The use of “punch” windows with consistent spacing around the building is encouraged to further this effect.

The use of trim at the cornice or various belt line heights will additionally serve to break up the wall area.

Storefront glazing is encouraged to minimize the vertical appearance by using “behind the glazing” vertical joints and articulated horizontal joints of “in front of the glazing” horizontal storefront. To further enhance this look, the spacing between the horizontal break-ups should be reduced to give a multiple rail effect. Where glazing occurs on both sides of a building corner, it is encouraged that the glazing actually meets at the corner and that right angle joint does not have a visible vertical mullion. The desired effect is to take a “notch” out of the buildings wall mass.

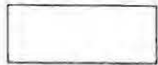
Since this is primarily an industrial warehouse dominated business park, it is desired that additional architectural elements be used to uniquely define the building’s use as something other than an ordinary office park. The way we have elected to create this uniqueness is through the use of “mass elements”. The mass elements are intended to be rectangular pop-outs along the building perimeter.

A contrast in building material or color is desired for mass elements as is a height differential to the building, rising above the parapet height to help break up the wall areas. The desire is to create a contrast in a bold architectural feature to the more refined look of wall reveals and belt lines.

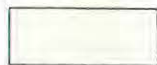
Concrete, block or plaster are desired materials for mass elements as it relates to the nature of the business use. Stucco in a contrasting finish to the building wall surface could also be used.

The goal for colors to be used for all buildings shall be earth tones. For illustration purposes, the graphics on the following pages indicate acceptable colors to be used. Additionally, the colors listed below, or comparables, are expressly acceptable:

DE5491 Glisten Green



DE5214 Tawny Amber



DEC714 Friar Tuck



DE5492 Silver Fern



DE5216 Autumn Umber



IV.A ARCHITECTURAL THEME (continued)

The use of canopies over doors and windows is also encouraged. The detailing should relate to the scale of the building. Inventive uses of steel and sustainable building materials are encouraged. It is desired that the look communicate a feeling that the people in this community are leaders. The buildings appearance should invite a second look that is not easily dismissed.

Ultimately the use of building materials in a way that creates a sense of play and interest is the desired outcome. It is a way to off set the heavy and somber look that could otherwise become a result.

EXTERNAL STRUCTURAL MATERIALS

Acceptable Materials

Concrete
CMU
Plaster/Stucco
Metal siding (accent only, not to exceed 33% of elevation)
Expanded or perforated metal
Metal decking/roofing
Storefront/curtain walls
Metal canopies/awnings
Metal trim elements

Unacceptable Materials

Faux stone or stone veneer
Wood siding or roofing
Residential wall claddings
Metal siding over 33% of elevation
Canvas or other fabrics
Tile wall coverings
Tile roof materials

BUILDING DESIGN

Acceptable Designs

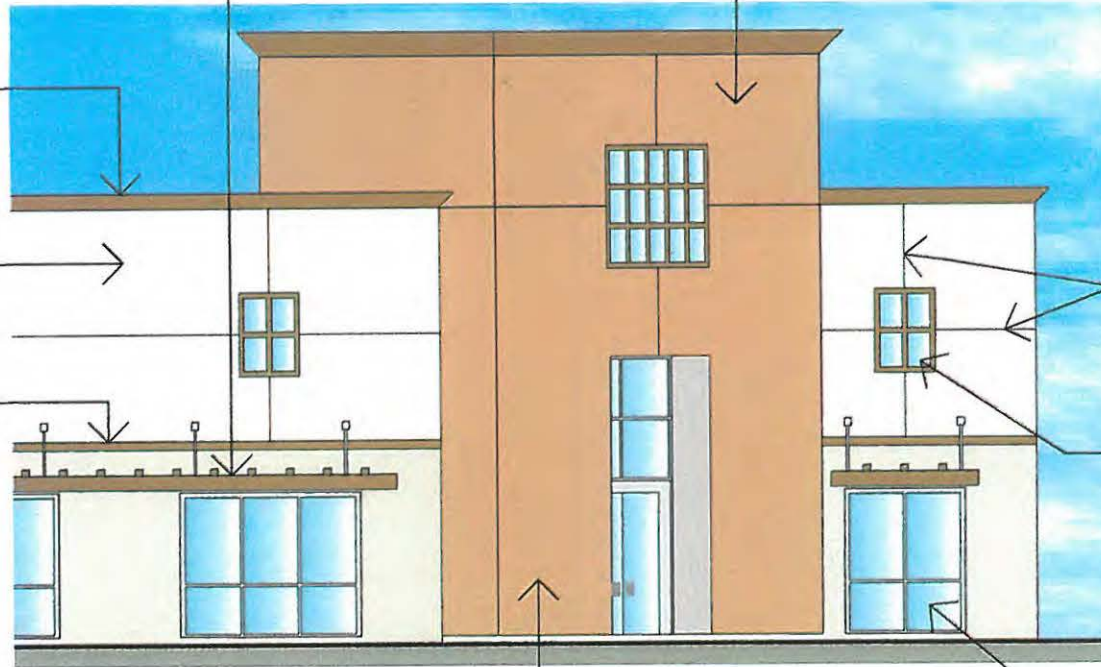
STEEL OR OTHER METAL CANOPIES OR AWNINGS USED IN SOME OR ALL OFFICE GLAZING AREAS. SEE DETAILS FOR ACCEPTABLE DESIGN.

SHAPED CROWN DETAIL, COLOR VARIATION FROM FLAT WALL SURFACES IS STRONGLY ENCOURAGED

CONCRETE TILT-UP OR CMU EXTERIOR WALLS ON MAIN BUILDING MASS

HORIZONTAL BANDING ELEMENTS - SCORING, REVEALS, OR APPLIED ELEMENTS TO PROVIDE BREAK IN WALLS. USE OF COLOR BREAKS IS STRONGLY ENCOURAGED

ARCHITECTURAL MASS ELEMENTS, TO POP OUT BEYOND MAIN BUILDING MINIMUM 48". SHAPES SHOULD BE NON-ORTHOGONAL. TRAPEZOIDAL SHAPES ARE ENCOURAGED - MASS ELEMENTS TO HAVE BOLD COLOR VARIATION FROM THE PRIMARY BUILDING, BUT ARE NOT TO BE GARISH AND SHOULD FALL WITHIN THE COLOR PALETTE PROVIDED IN THIS SPECIFIC PLAN. ACCEPTABLE MATERIALS ARE STEEL DECKING, STUCCO, OR CMU; EITHER IN A CONTRASTING TEXTURE TO THE MAIN BUILDING



WRAP MASS ELEMENT OVER AND BEYOND PARAPETS (MIN. 48" BEYOND PARAPET HORIZONTALLY)

SCORING, REVEALS OR APPLIED ELEMENTS TO BREAK UP ELEVATIONS HORIZONTALLY AND VERTICALLY

APPLIED OR REAL GLAZING ELEMENTS, OR REVEALS, SCORING OR APPLIED ELEMENTS TO SIMULATE WINDOWS ON WALLS THAT ARE GREATER THAN 18' ABOVE FINISH FLOOR

STOREFRONT ELEMENTS TO INCLUDE INTERMEDIATE MULLIONS FOR VISUAL RELIEF

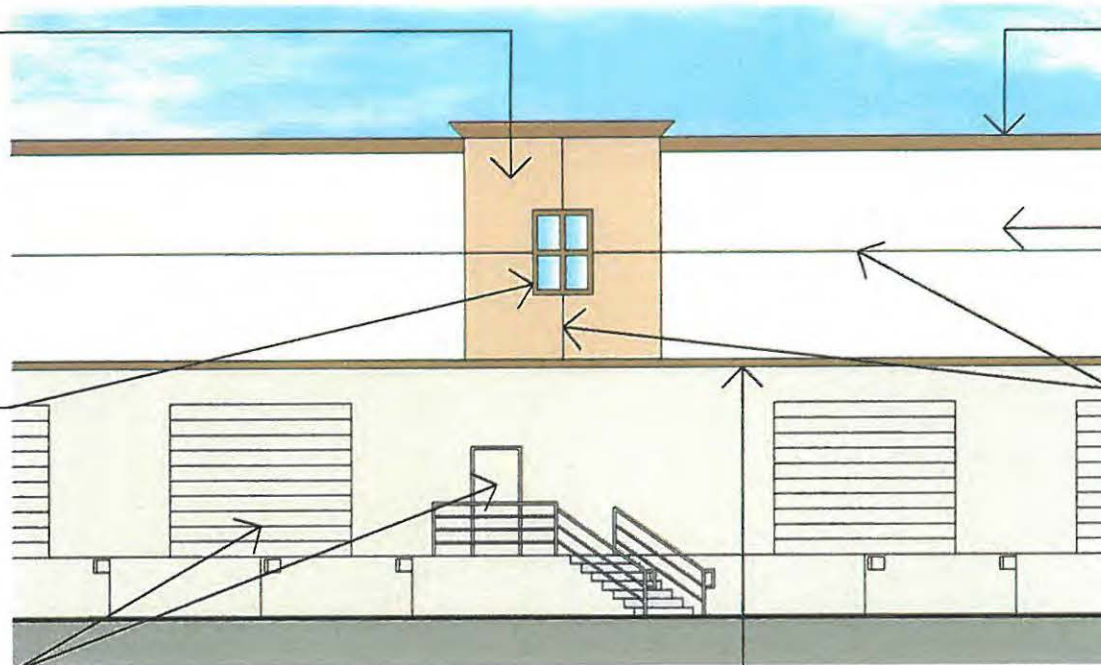
Exhibit 4-A

Acceptable Designs

PROVIDE SECONDARY MASS ELEMENT TO BREAK LARGE BUILDING MASSING AND REDUCE THE SCALE OF THE OVERALL STRUCTURE TO A MORE PEOPLE-FRIENDLY LEVEL CONTRASTING / VARYING COLOR SHOULD BE PROVIDED

APPLIED OR REAL GLAZING ELEMENTS, OR REVEALS, SCORING OR APPLIED ELEMENTS TO SIMULATE WINDOWS ON WALLS THAT ARE GREATER THAN 18' ABOVE FINISH FLOOR

TRUCK / LOADING / DOCK DOORS AND METAL MAN DOORS SHOULD BE PAINTED OR FINISHED TO MATCH OR VARY ONLY SLIGHTLY FROM ADJACENT BUILDING COLORS



SHAPED CROWN DETAIL, COLOR VARIATION FROM FLAT WALL SURFACES IS STRONGLY ENCOURAGED

CONCRETE TILT-UP OR CMU EXTERIOR WALLS ON MAIN BUILDING MASS

SCORING, REVEALS OR APPLIED ELEMENTS TO BREAK UP ELEVATIONS HORIZONTALLY AND VERTICALLY

HORIZONTAL BANDING ELEMENTS - SCORING, REVEALS, OR APPLIED ELEMENTS TO PROVIDE BREAK IN WALLS. USE OF COLOR BREAKS IS STRONGLY ENCOURAGED

Exhibit 4-B

BUILDING DESIGN (continued)

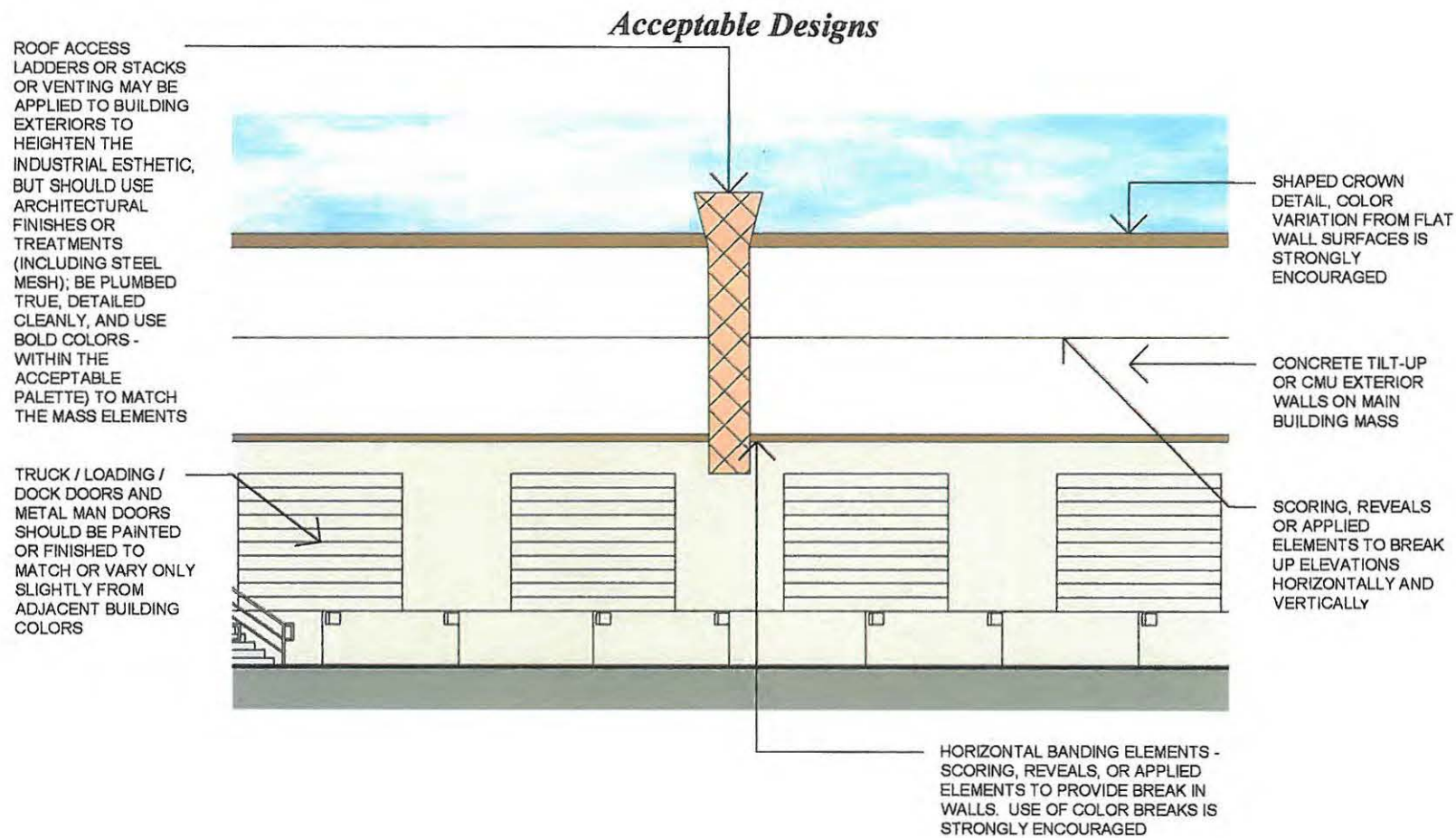


Exhibit 4-C

BUILDING DESIGN (continued)

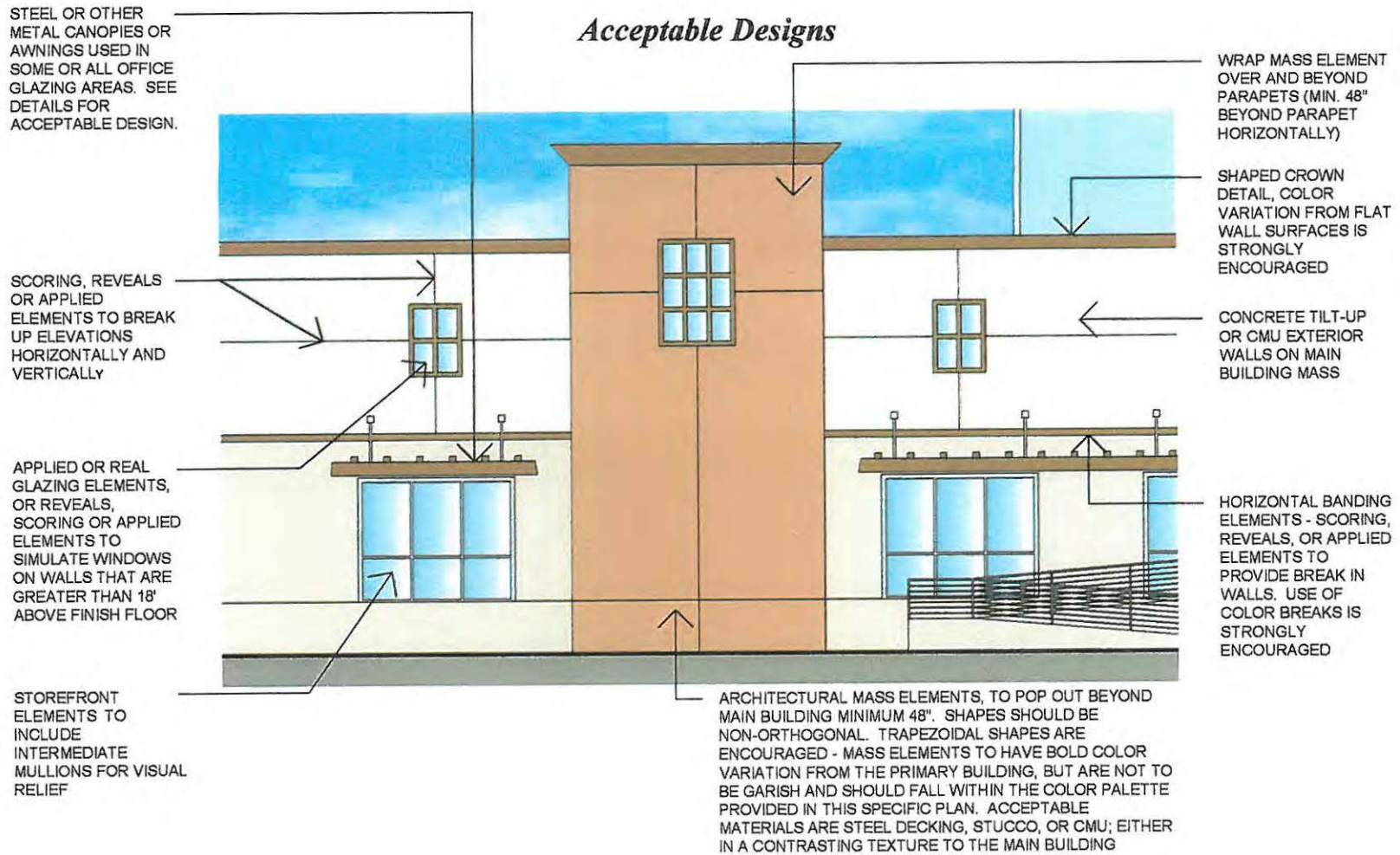


Exhibit 4-D

Acceptable Designs

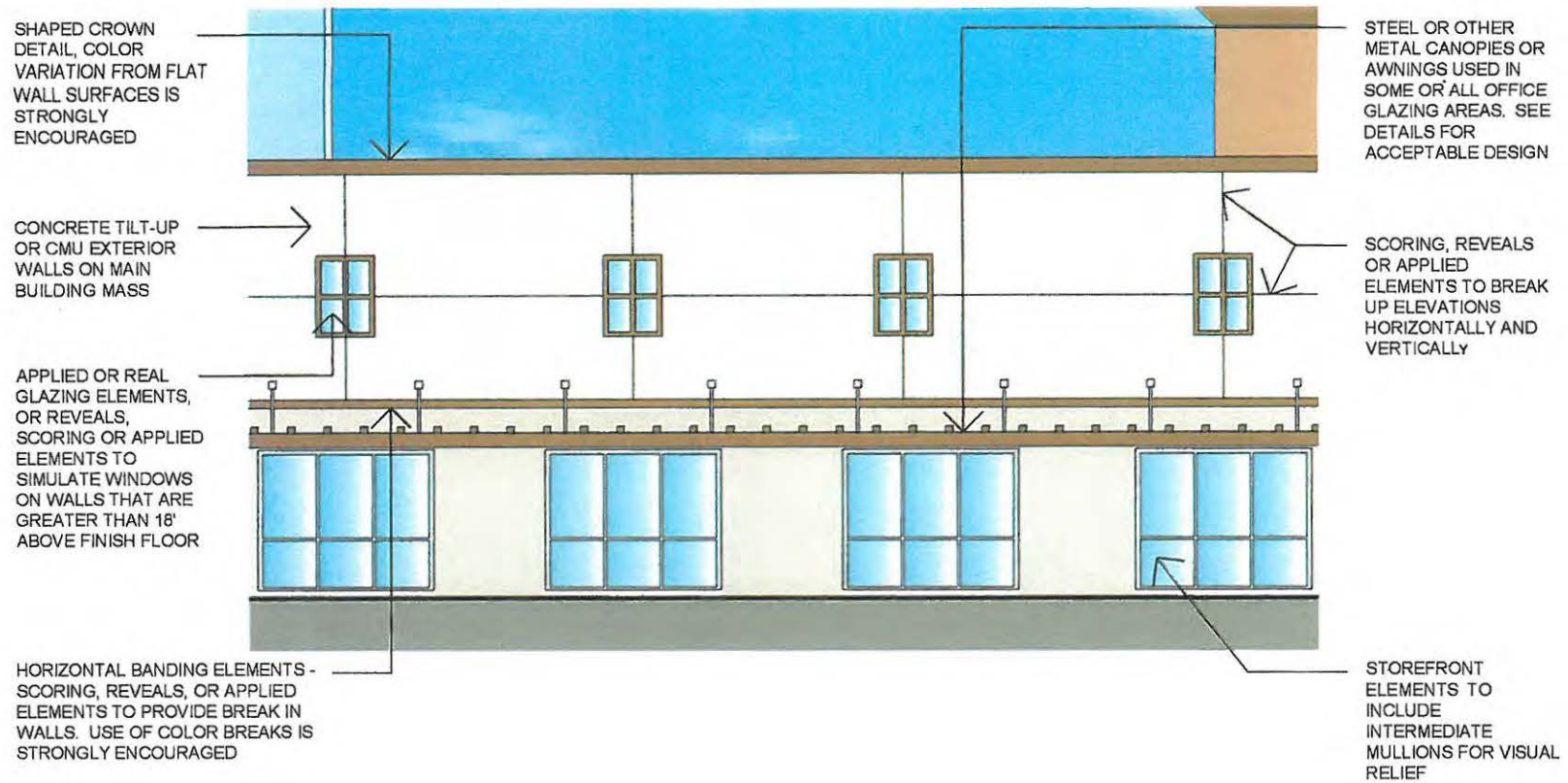


Exhibit 4-E

Acceptable Designs

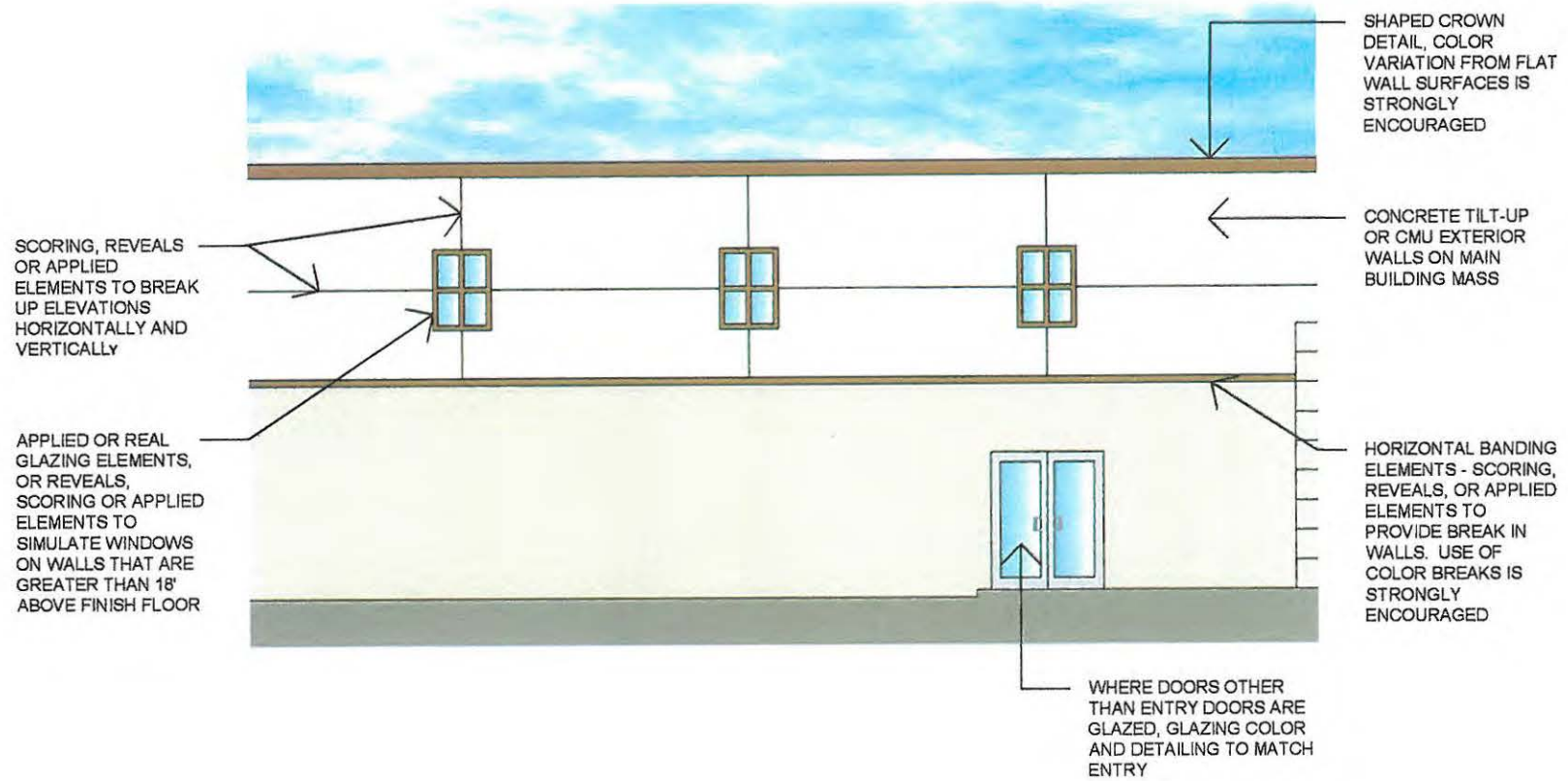


Exhibit 4-F

Unacceptable Designs

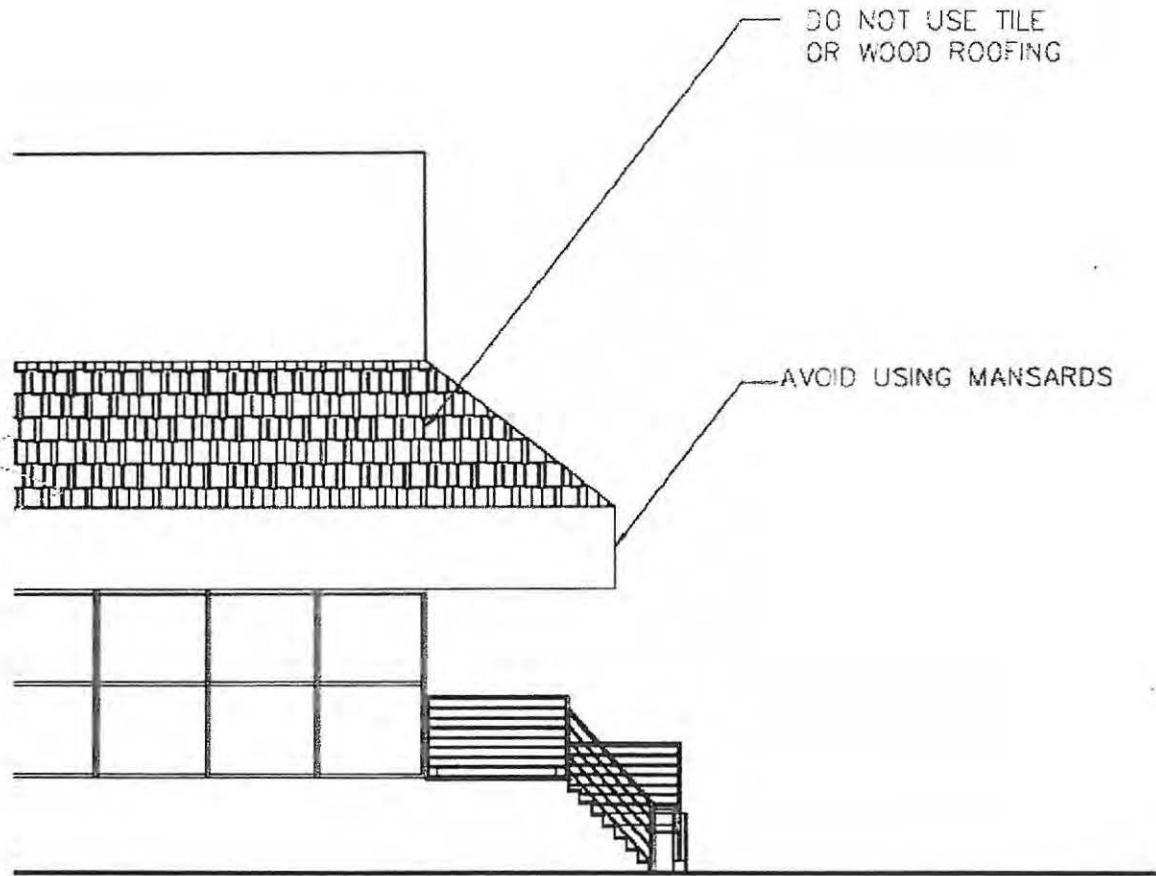
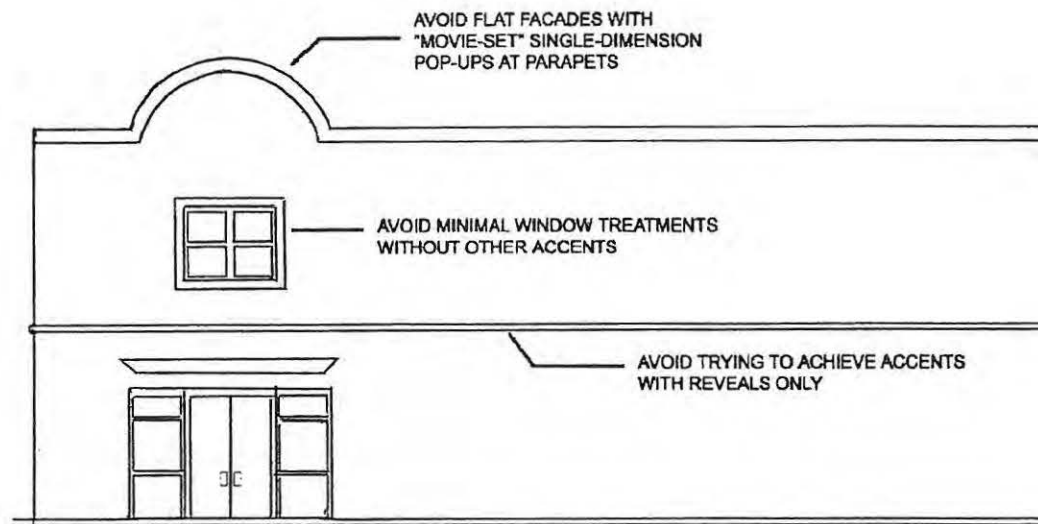


Exhibit 4-G

Unacceptable Designs



SECTION FIVE

SITE STANDARDS

V.A SITE GOALS

It is the goal of these Site Standards to present a professional, attractive, safe, inviting and unified theme and appearance to Mission – 71 Business Park. The standards presented herein are minimum standards and may be augmented by future developers of the Project.

These standards incorporate all of the visual elements that make up the non-building esthetic of the Project. It is the desire of the Owner and the City of Pomona that this primary western gateway into the City present an image of a vibrant, healthy business community. In addition to providing for visual comfort, these standards will provide for adequate parking, loading and circulation to encourage quality businesses to locate in the Project, create the image of a vibrant, corporate community in the City, and discourage users whose activities would hurt adjoining values and the City's image. Maintaining a non-congested Project is a key element of the attractiveness of the Project to the caliber of users the Owner and the City of Pomona envision.

V.B OVERALL SITE DEVELOPMENT STANDARDS

SETBACKS:

- Mission Boulevard: 25' minimum landscape setback from property line to building or parking areas, including a minimum 4' high landscape berm and 8' screen wall.
Minimum 5' landscaped setback between building and parking except at loading areas or unless screened by a minimum 8' high wall, facing Mission Boulevard.
- Second Street: 15' minimum landscape setback from curb line to building or parking areas.
Minimum 5' landscaped setback between building and parking except at loading areas.
- Curran Place: Minimum 10' landscaped setback from building or parking areas and property line.
- Interior of lots: Minimum 5' landscaped setback between building and parking facing a street, or within 50' of a street.
- Side or rear yards: Minimum 5' landscaped setback along property lines, except no setback required if building is immediately adjacent to a property line (within 12").
- 71 Corona Expressway: Minimum 10' landscaped setback along property line. Note: this can be reduced or eliminated if Caltrans provides a landscape buffer along the improvements proposed for the Expressway, and shall be subject to Planning Division review.

V.B OVERALL SITE DEVELOPMENT STANDARDS (continued)

BUILDING HEIGHTS:

Maximum allowable building height to be 60' or 4 stories, whichever is greater.

MINIMUM REQUIRED PARCEL WIDTHS:

The minimum required parcel widths shall be 100'. For triangular parcels, the average overall width in any direction should be 120'.

MINIMUM REQUIRED PARCEL AREA:

The minimum required parcel area shall be 0.8 acres (34,848 s.f.) except shall be 20,000 s.f. for parcels 10, 14, 19, 20, 23 and 24. Adjustments to this can be made with Planning Division approval.

MINIMUM REQUIRED LANDSCAPING:

The minimum required landscaping on a parcel shall be 6% of the parking area, excluding the front landscape setback. The front landscape area shall not count towards this 6% minimum.

V.C PARKING STANDARDS

Current City of Pomona parking standards do not provide for the tiered parking needs for large-area distribution Tenants. The Specific Plan proposes to rectify this by creating a tiered parking standard consistent with those used by the City of Chino, a neighboring City to the south of Pomona that is the home of a large inventory of distribution warehouses. The City of Chino's parking standards for distribution uses have proven to be successful and are used as the model for these standards, but similar tiered standards for warehouse and distribution uses are found in many southern California cities. The office requirement reflects a similar allowance for limited office space included in the main building requirements, up to designated limits. The Specific Plan parking standards are:

AUTOMOBILES:

Offices:	Same as current City standards:	4 per 1,000 s.f.
Manufacturing:	Same as current City standards:	2 per 1,000 s.f.
Warehouse:	0 s.f. to 20,000 s.f.	1 per 1,000 s.f.
	20,001 s.f. to 40,000 s.f.	20 spaces plus 1 space per each additional 2,000 s.f. over 20,000 s.f.
	40,000 s.f. and up:	30 spaces plus 1 space per each additional 4,000 s.f. over 40,000 s.f.
Other uses:	Same as current City standards.	

V.C PARKING STANDARDS (continued)

AUTOMOBILES (continued):

Stall sizes:

Standard stalls:	9'-6" x 19'	
Compact stalls:	8' x 16'	
Parallel stalls:	10' x 24'	(these are discouraged)
Handicap stalls, auto:	14'-6" x 19'	
Handicap stalls, van-accessible:	17'-6" x 19'	

Note: for any use that provides more than ten parking spaces, a maximum of twenty-five percent of the spaces in excess of ten may be compact. All compact spaces shall be clearly marked: "Compact Only".

Drive aisles: Same as current City standards, except minimum 28' for 90° parking and for any other aisle that serves as Fire Department access lanes.

Striping: All stalls shall be double-striped per City of Pomona standards.

BICYCLES:

Bicycle parking rack spaces shall be provided at one space for each 50 building occupants.

V.C PARKING STANDARDS (continued)

TRUCK LOADING STANDARDS

No trucks shall be permitted to back in from the public right-of-way. All sites shall be designed so that no truck loading occurs in primary drive aisles. Loading may occur on an individual lots internal drive aisle, or on a shared drive aisle that does not block primary access out of a lot. All new structures shall comply with the following:

0 to 15,000 s.f.:	Side loading:	minimum one loading door, loading space of 12' x 30', okay to block a secondary access to primary drive aisles or public way, shall not block only access route for another parcel.
	End loading:	no requirement.
15,001 s.f. and above:	Side loading:	minimum one loading door, loading space of 12' x 30', okay to block a secondary access to primary drive aisles or public way, shall not block only access route for another parcel. Side loading door may be replaced by end loading door as noted below.
	End loading:	one loading door for each 20,000 s.f. of gross building area, loading space of 12' x 53', unless user can show these requirements are excessive; shall not block any drive aisles.
Maneuvering:	Side loading:	maneuvering must be adequate for a 28' long delivery truck to back in and out of.
	End loading:	maneuvering must be adequate for a 53' trailer with a tractor to maneuver. Clear room for 90° docks to be minimum 120' from dock face to any obstruction, including parking stalls, for dock positions 12' on center, 115' for dock positions 14' on center, 110' for dock positions 18' or more on center. Angled or saw-tooth docks to be as approved by the engineering department, but to provide a minimum 95' clear space. Regardless of these requirements, adequate truck maneuvering must be demonstrated, including clear area for wheel tracking of trailers.

V.D WALLS / SCREENING

TRASH ENCLOSURES

- All trash areas shall be enclosed with a 6' high decorative cmu or concrete tilt-up wall (see Exhibit 5-A pg 5.5) and solid metal gates (corrugated or other material to match building accent panels), painted to match wall or in approved accent colors consistent with the main building color palette. Overhead trellises, steel or other metals, may be provided. Design elements will match canopies or awnings on the main building and shall be painted to match walls or in approved accent colors. Trash enclosures shall be a minimum of 7'x 10'.
- Vines shall be planted and trained onto enclosure walls at 6' o.c. (see pg 5.11).

LOADING DOCK AND PARKING AREAS

- All loading dock areas are to be screened from view with a minimum 8' concrete wall (see pg 5.12) on all new developments when facing a public way.
- Parking areas are to be screened from streets with a 4' concrete wall (see pg 5.12) and/or screening shrubs (see pg 5.11).

PROPERTY LINE WALLS

- If desired, property line walls shall be a minimum of 6' concrete walls (see pg 5.12).

TRASH ENCLOSURE

Notes:

- 1) Walls to be decorative cmu or concrete tilt-up.
- 2) Main receptacle access doors and tenant access door to be solid steel or other metal; corrugated or ribbed. Paint to match enclosure walls or to match main building accent panel palette.
- 3) Access door to have 84" minimum opening.
- 4) Each enclosure to receive minimum two refuse bins.

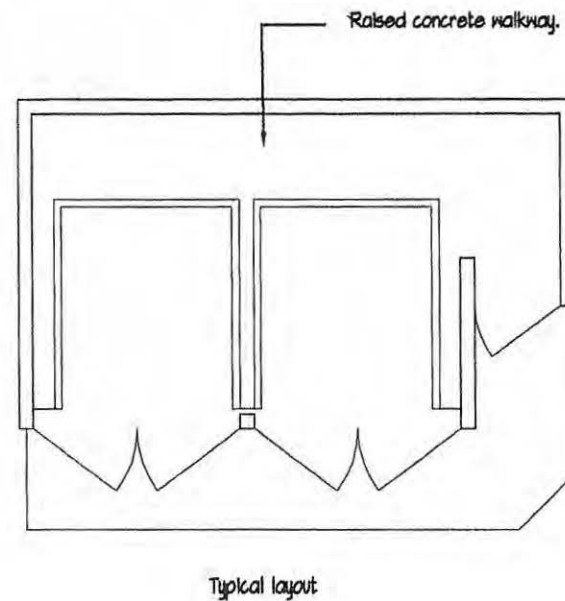
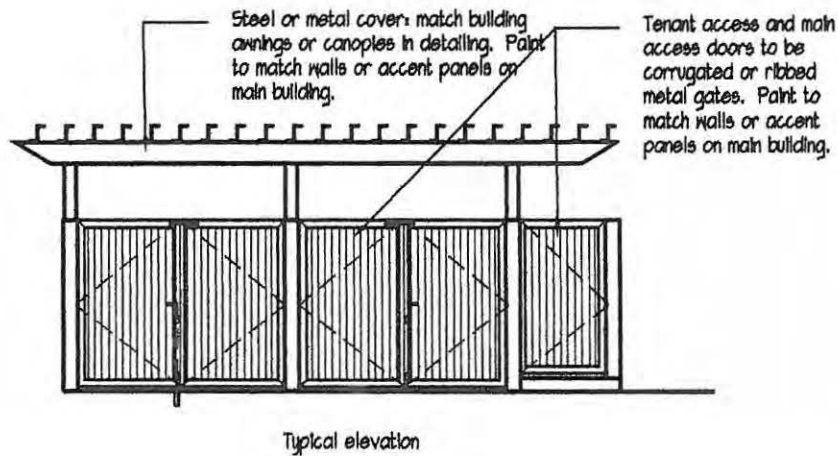


Exhibit 5-A

V.E CIRCULATION AND PARKING DESIGN

ACCESS TO PARCELS:

- Entry roads that lead to collector roads must be approved by the Pomona Planning Division and Public Works Department. Obstructions such as landscaping, signs or walls exceeding 2 feet in height are not permitted within the “site triangle” in order to keep visibility at intersections safe (see Exhibit 5-B).
- Entries shall create a clear sense of arrival. Special landscape features and sign structures are encouraged as they create orientation for visitors. Exhibit 27 shows a typical entrance with suggested elements that identify this access as a main entry point.

ROAD HIERARCHY:

Ingress and egress points shall be designed to minimize hazards, inconvenience, and congestion by providing simple circulation patterns, ample stacking room, and ease of observation for visitors.

GENERAL PARKING DESIGN:

- On street parking is prohibited.
- Parking facilities must comply with applicable City of Pomona ordinances, except as amended herein.
- Concrete wheelstops are required adjacent to all landscape planters, unless the planter is 10’ deep or greater.
- Screening between adjoining lots shall include walls and/or tree and shrubs, if applicable.

V.E CIRCULATION AND PARKING DESIGN (continued)

GENERAL PARKING DESIGN (continued):

- Accommodations shall be made for visitor parking and drop off facilities are encouraged in connection with main building entries, as appropriate to individual use.
- Per the Americans with Disabilities Act (ADA), handicapped accessibility shall be incorporated for all buildings.

OUTDOOR STORAGE:

- No outdoor permanent storage is allowed that is visible from a public right-of-way or internal circulation drive. If outdoor storage is necessary, it must be fully screened within (and may not be higher than) a wall that matches or relates to the main building in style and material.

PAVING SURFACES:

- All loading docks, driveways, entrances, and parking lots shall be paved with concrete, asphalt or other hard surface material suitable for the intended use, in compliance with City of Pomona standards and specifications..

PARKING DESIGN - SITE TRIANGLES

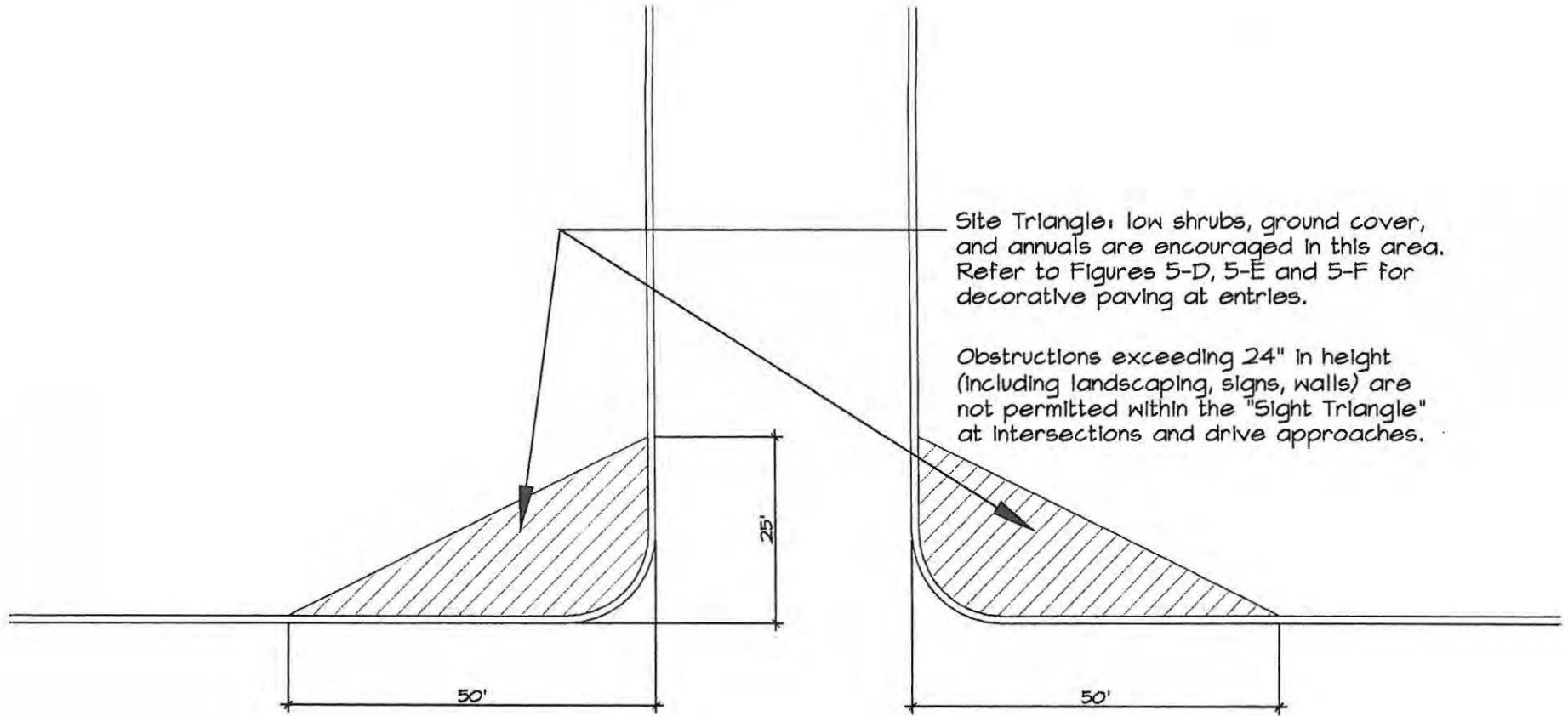


Exhibit 5-B

PARKING DESIGN - LANDSCAPE PLANTERS

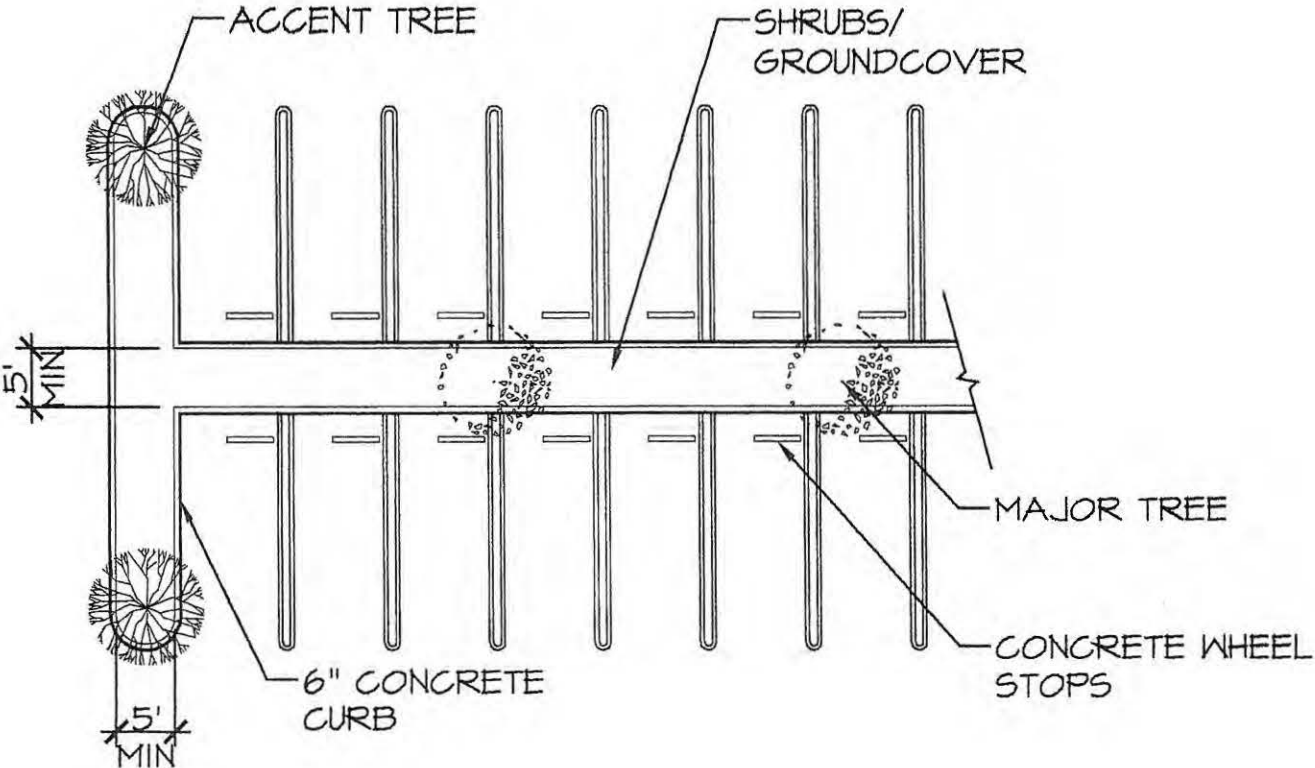
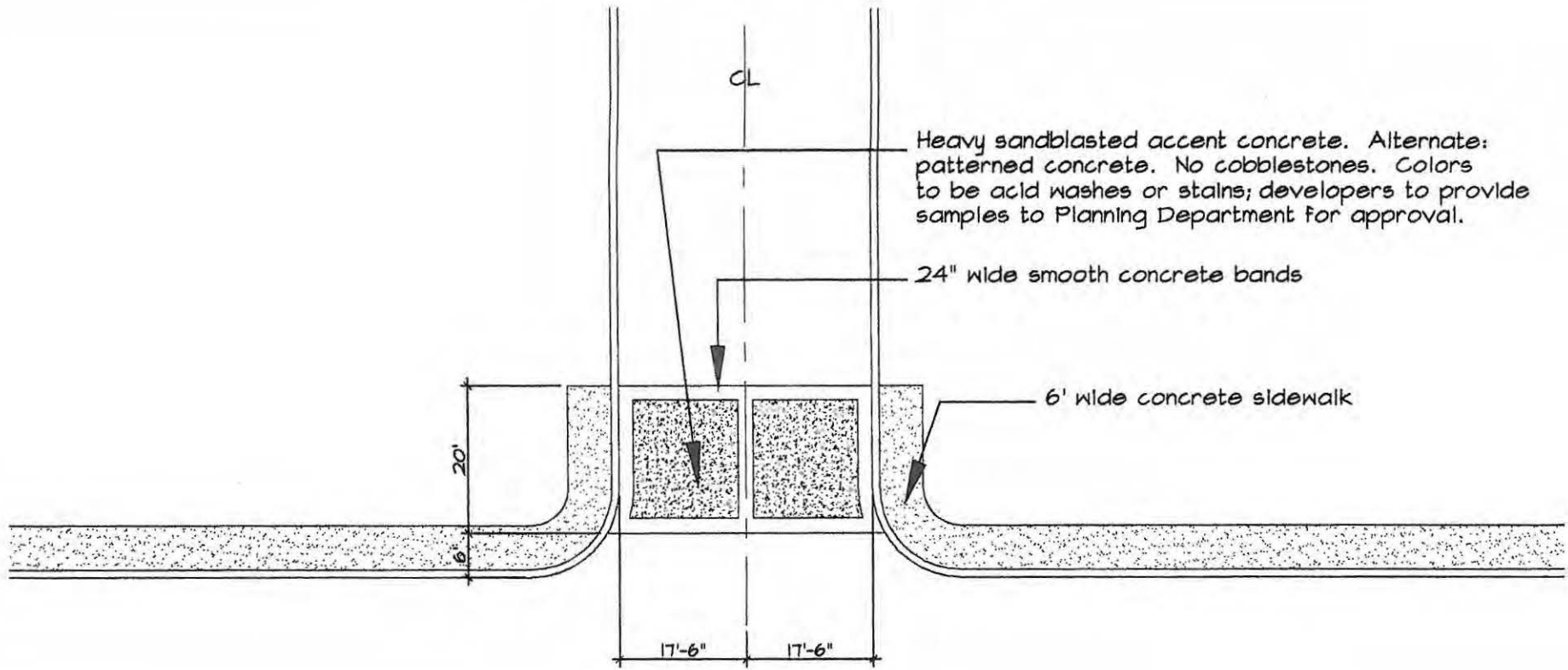


Exhibit 5-C

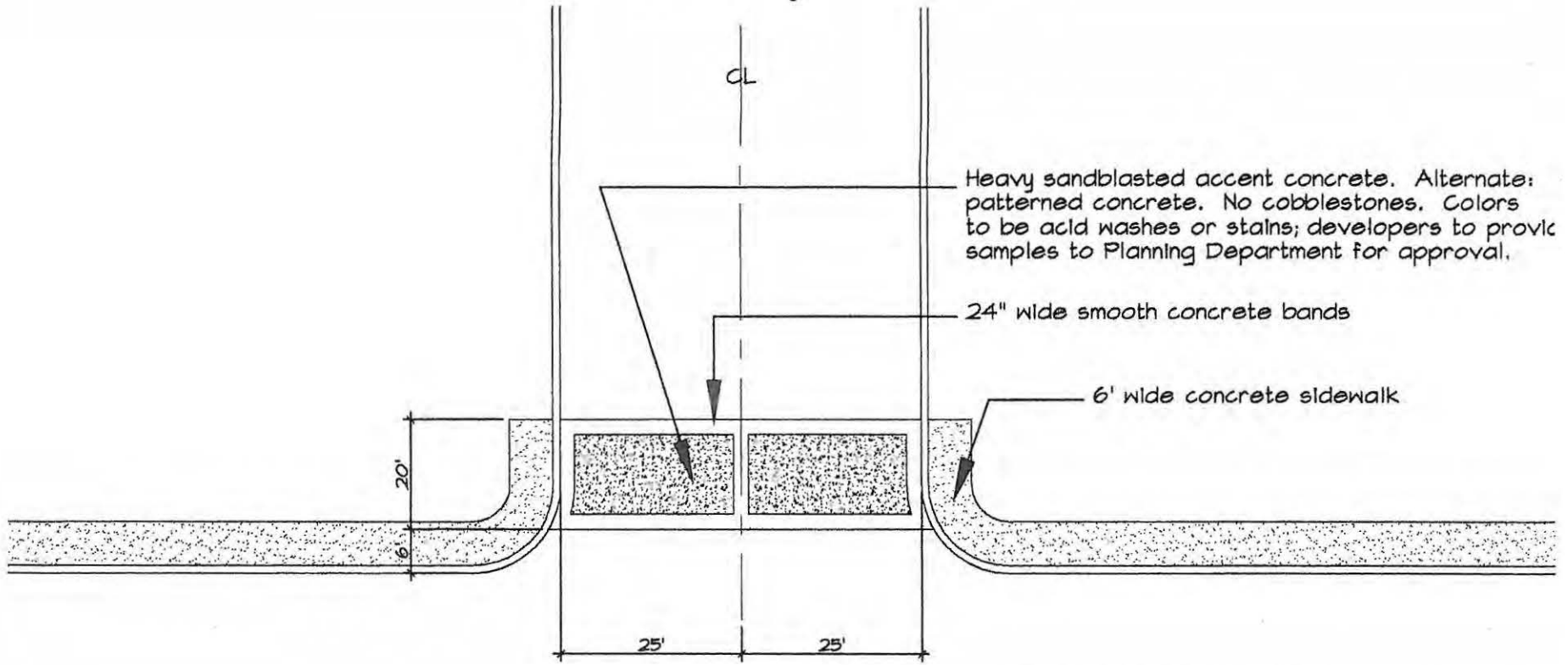
CIRCULATION ENTRY – 35' WIDE



REFER TO EXHIBIT 5-J

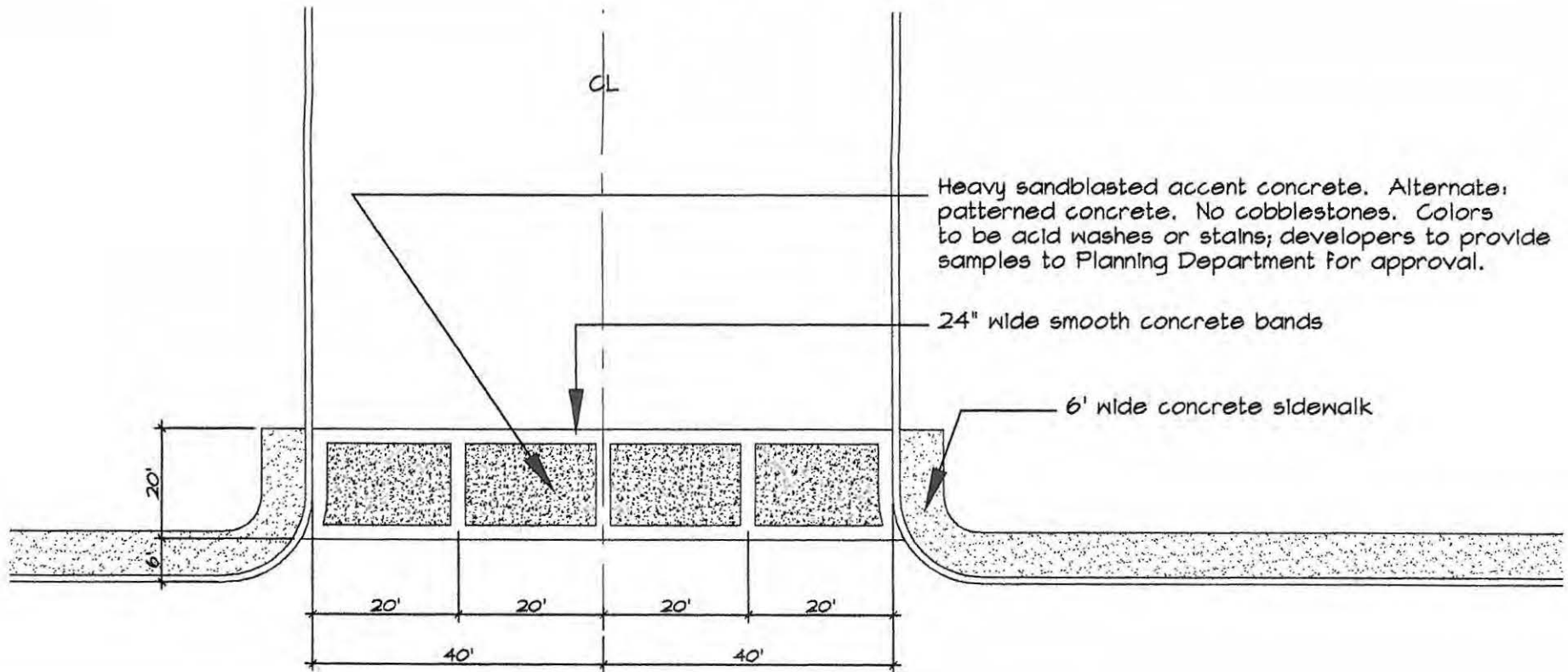
Exhibit 5-D

CIRCULATION ENTRY – 50' WIDE



REFER TO EXHIBIT 5-J

CIRCULATION ENTRY – 80' WIDE



REFER TO EXHIBIT 5-J

MASTER SITE VISIBILITY PLAN

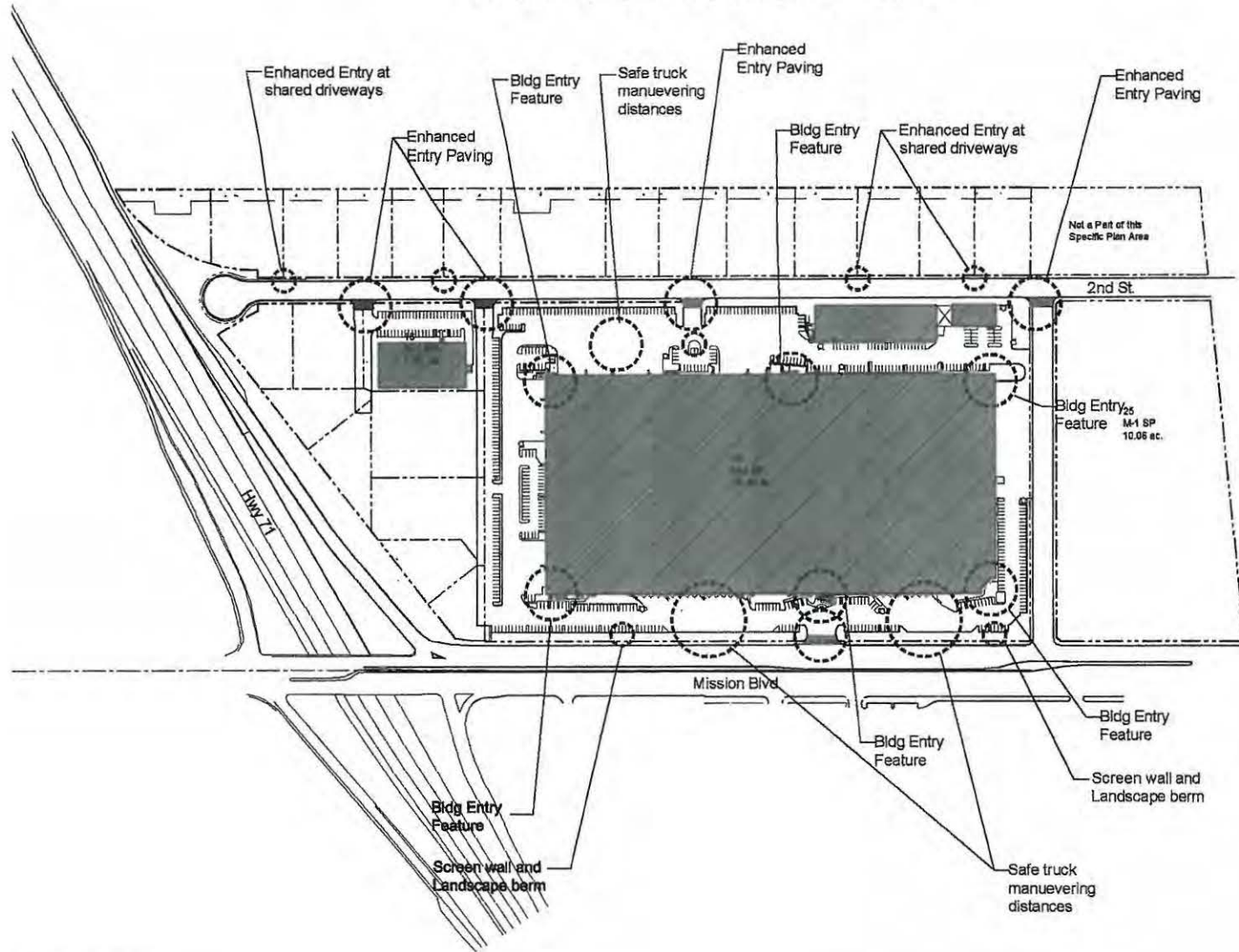


Exhibit 5-G

EXISTING PROJECT CIRCULATION PLAN

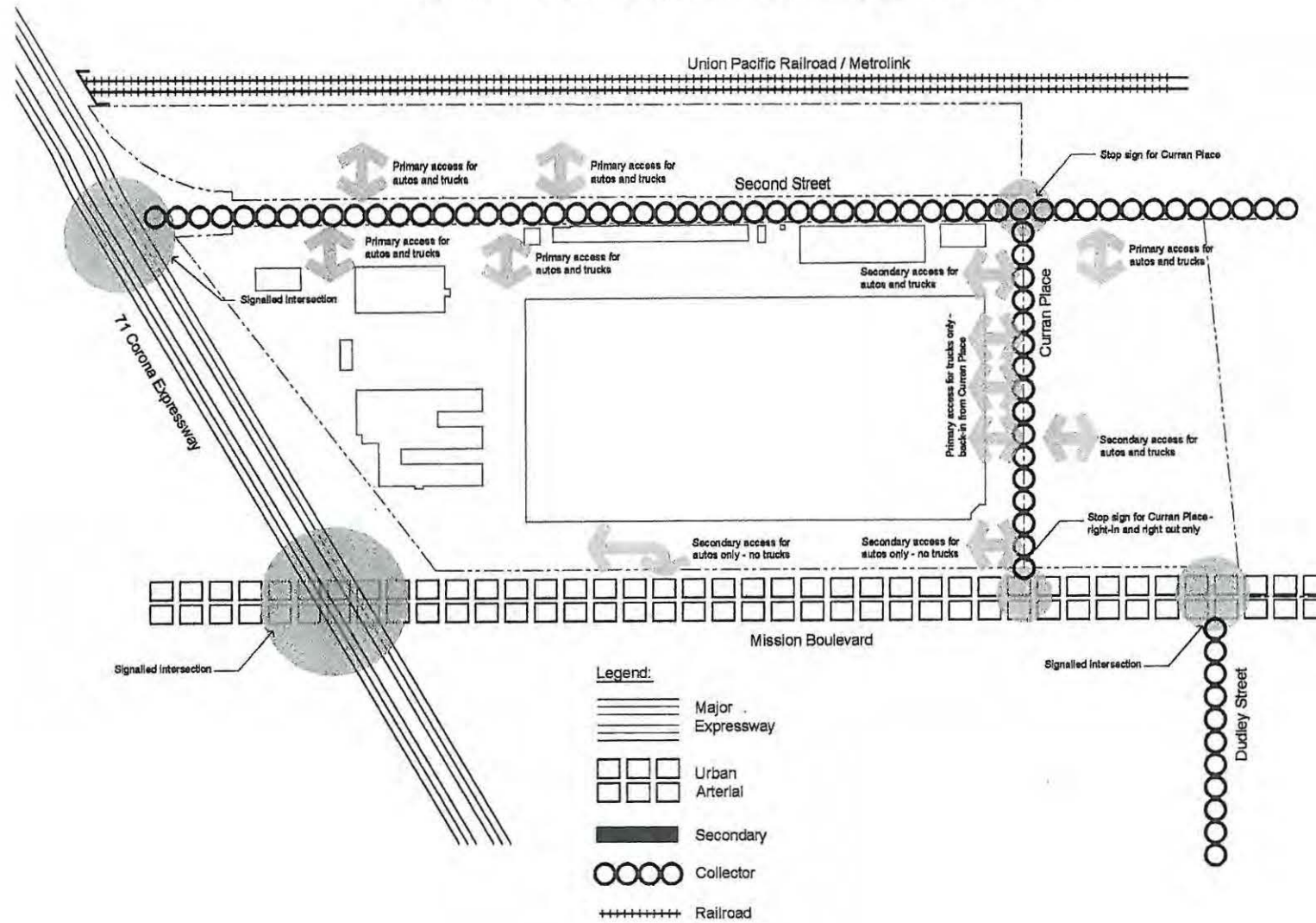


Exhibit 5-H

FUTURE PROJECT CIRCULATION PLAN

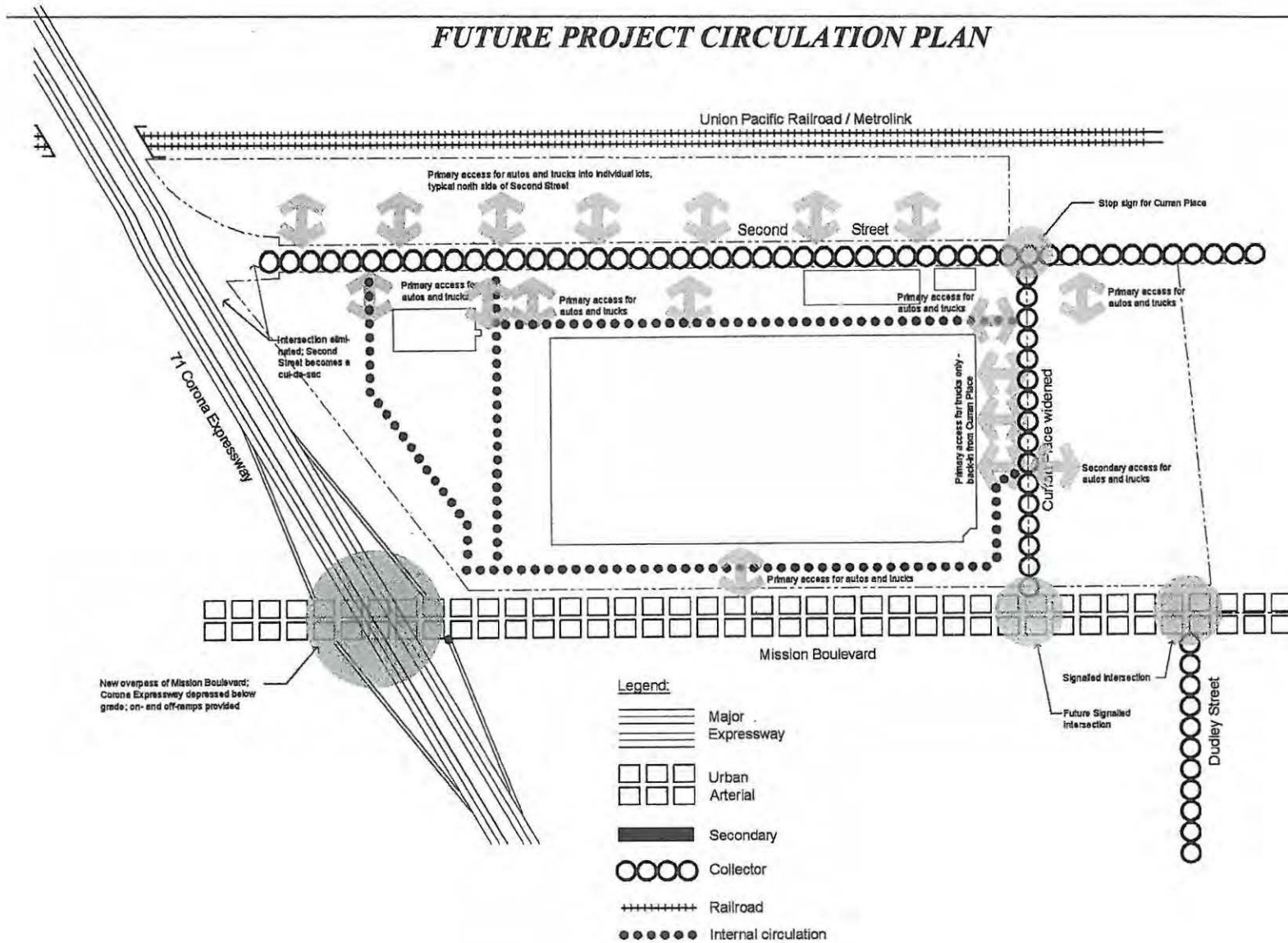


Exhibit 5-J

V.F LANDSCAPE DESIGN

Landscape requirements promote the image of the development, safe guard and enhance property values, and minimize the visual impact of buildings and parking lots. These requirements are detailed below.

- All building perimeters and building entrances shall be appropriately landscaped. Entrances shall be accentuated by the landscape. These planting areas shall be a minimum of 5' wide and surrounded by a 6" concrete curb.

Note: This requirement shall apply to all new structures built after this Specific Plan is approved.

- Trash enclosures must be screened with a 6' high decorative cmu or concrete tilt-up walls (see Exhibit 5-A pg 5.5) with vines or shrubs trained on wall.
- All new HVAC and other mechanical equipment shall be screened with a parapet or approved screen walls so that the equipment is not visible from the public right-of-way. All existing HVAC or other mechanical equipment located on existing structures shall be either screened or painted in complimentary or approved accent colors with the building. These guidelines will not apply to visibility from elevated sections of Mission Boulevard and the 71 Corona Expressway. If HVAC or other mechanical equipment is located on the ground, not within an enclosed building, it must be screened by plant material or concrete wall.
- Parking trees shall be 1 tree per 5 parking spaces, except in truck loading areas.

STREET TREES & LANDSCAPE SETBACK

MISSION BOULEVARD AT BERM

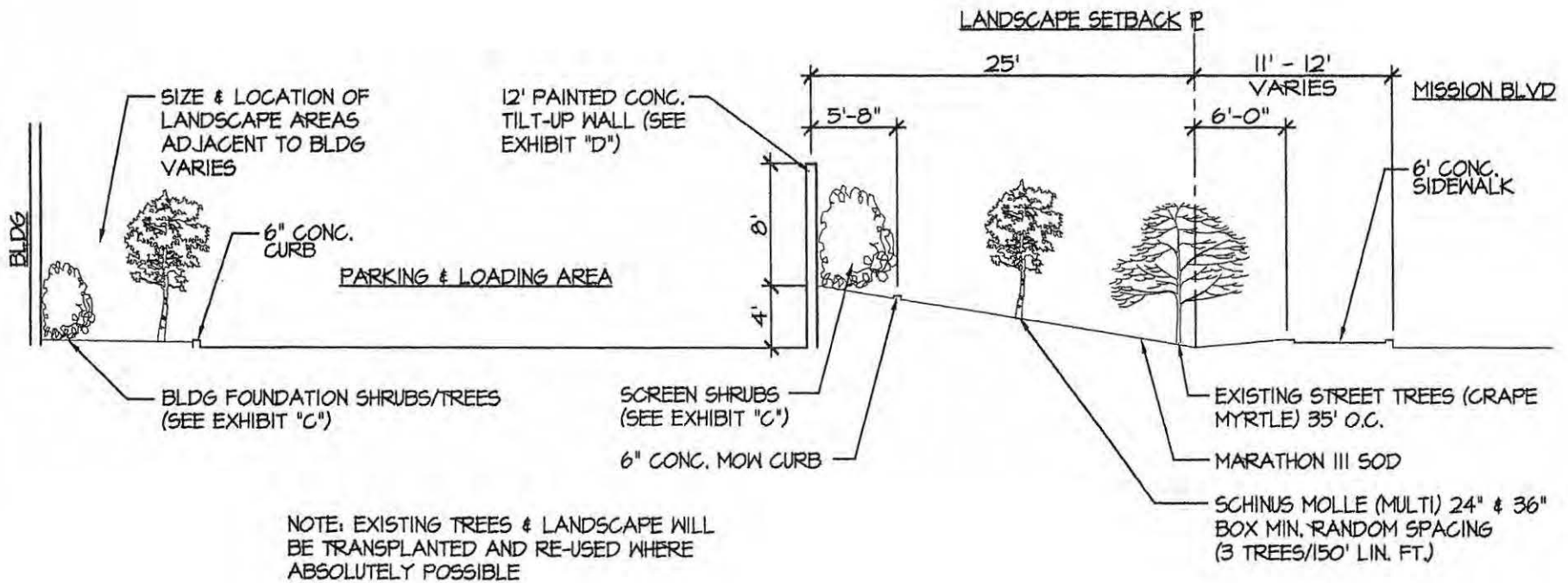


Exhibit 5-K

STREET TREES & LANDSCAPE SETBACK

MISSION BOULEVARD – AWAY FROM BERMS

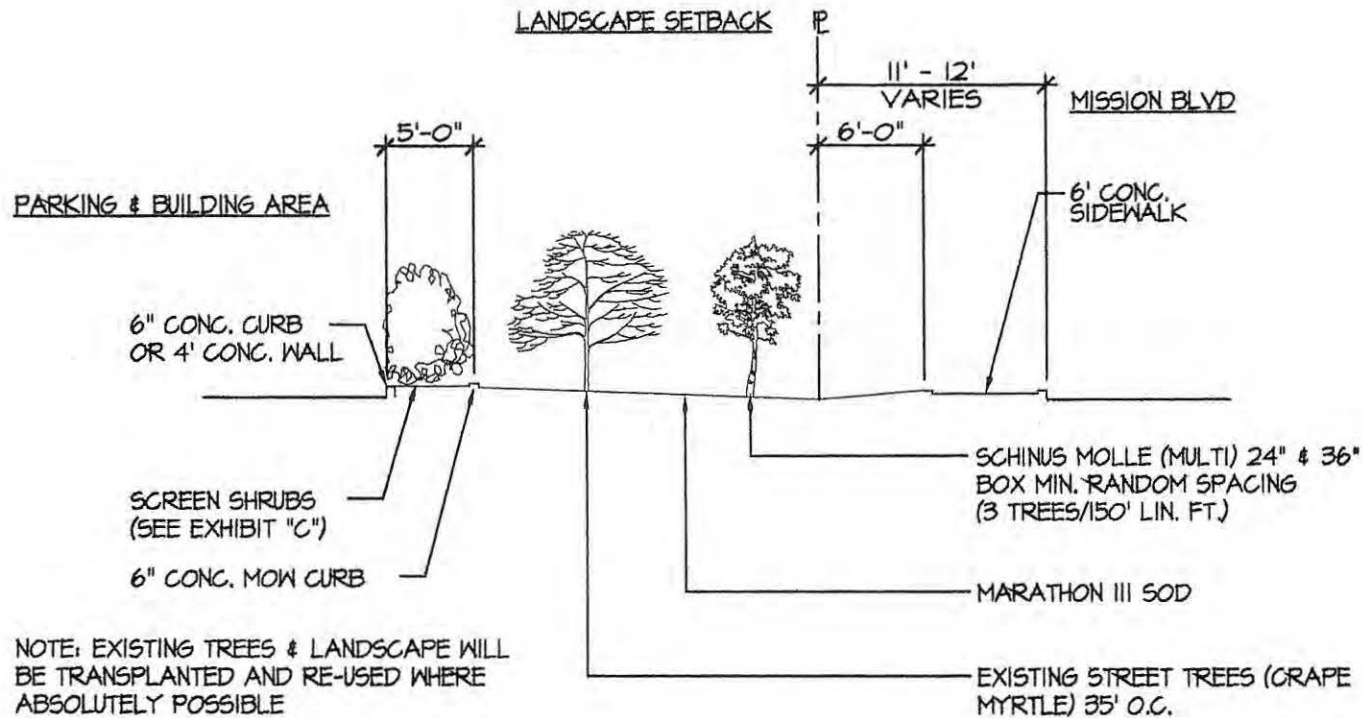


Exhibit 5-L

STREET TREES & LANDSCAPE SETBACK

CURRAN PLACE

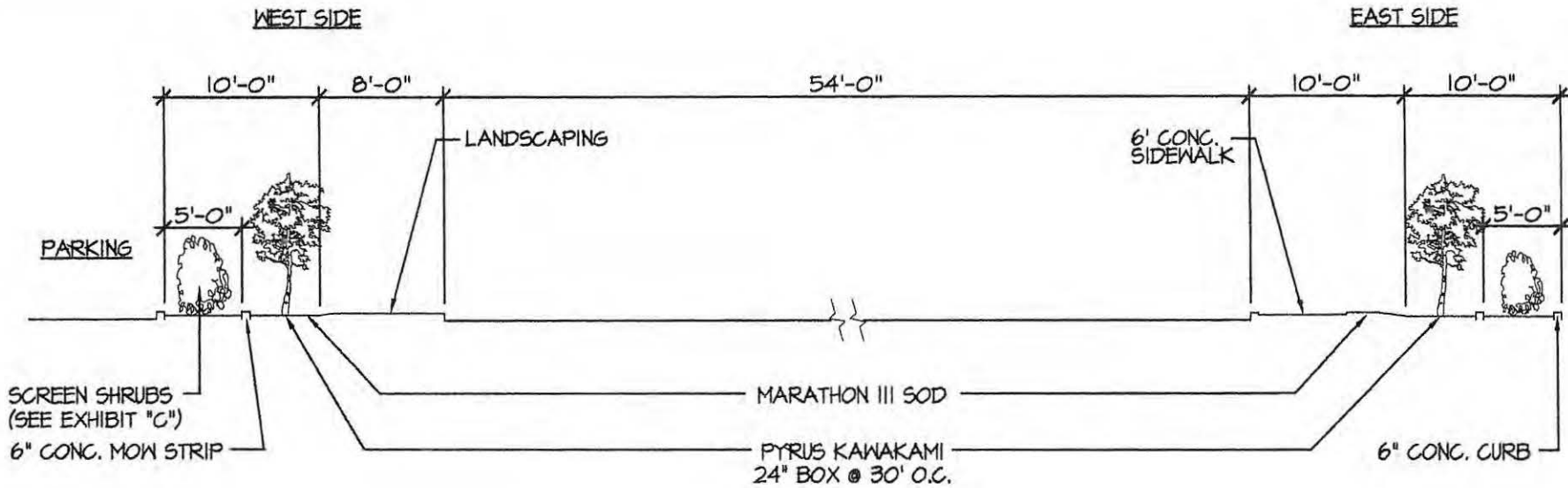


Exhibit 5-M

STREET TREES & LANDSCAPE SETBACK

SECOND STREET – NORTH SIDE

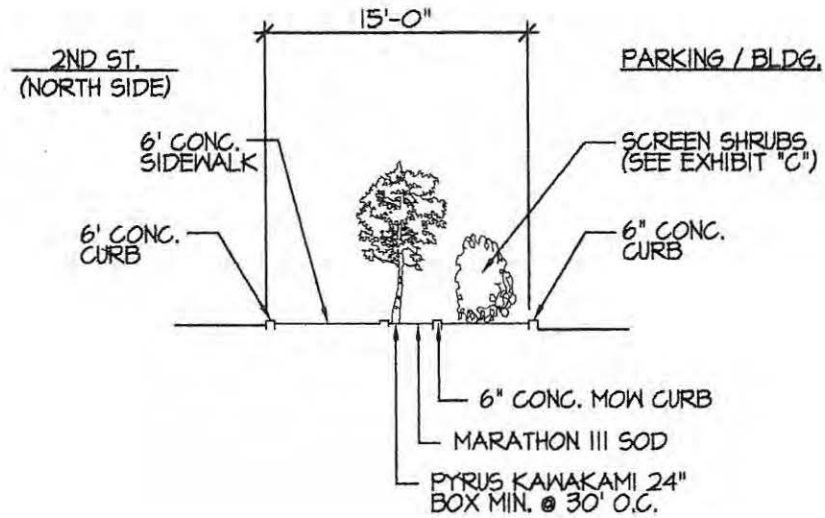


Exhibit 5-N

STREET TREES & LANDSCAPE SETBACK

SECOND STREET – SOUTH SIDE

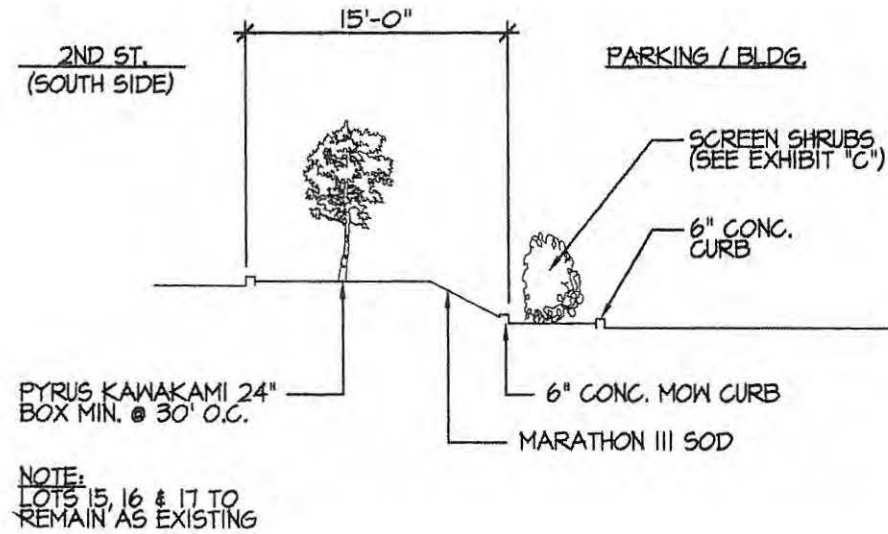
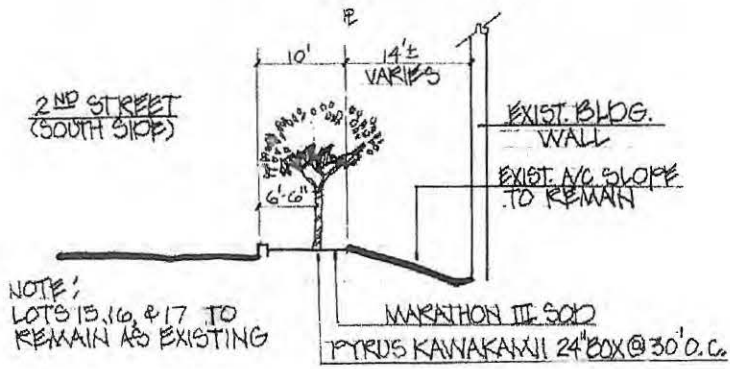


Exhibit 5-O

STREET TREES & LANDSCAPE SETBACK

SECOND STREET – SOUTH SIDE AT EXISTING BUILDINGS 7 AND 28



V.G PLANTING LIST: PARKING LOT

<u>BOTANICAL NAME</u>	<u>COMMON NAME</u>	<u>MINIMUM SIZE</u>
<i>Trees</i>		
Prunus c. "Krauter Vesuvius" (use as accent)	Purple Leaf Plum	24" box
Quercus ilex (major parking lot tree)	Holly Oak	24" box
<i>Shrubs</i>		
Rhaphiolepis "Ballerina"	Indian Hawthorne	5 gallon
<i>Groundcover</i>		
Rosmarinus o. "Prostratus"	Rosemary	1 gallon @ 3' o.c.
Trachelospermum jasminoides	Star Jasmine	1 gallon @ 3' o.c.

V.H PLANTING LIST: TREE, SHRUB AND GROWDCOVER ADJACENT TO BUILDING

<u>BOTANICAL NAME</u>	<u>COMMON NAME</u>	<u>MINIMUM SIZE</u>
<i>Trees/Palms</i>		
Cercis canadensis	Eastern Redbud	Mix of 15", 24", 36" box
Chamaerops humilis	Mediterranean Fan Palm	Mix of 15", 24", 36" box
Cinnamomum camphora	Camphor Tree	Mix of 15", 24", 36" box
Geijera parviflora	Australian Willow	Mix of 15", 24", 36" box
Maytenus boaria	Mayten Tree	Mix of 15", 24", 36" box
Phoenix canariensis	Canary Island Date Palm	Mix of 15", 24", 36" box
Phoenix dactylifera	Date Palm	Mix of 15", 24", 36" box
Phoenix roebelenii	Pygmy Date Palm	Mix of 15", 24", 36" box
Pinus canariensis	Canary Island Pine	Mix of 15", 24", 36" box
Pyrus kawakami	Evergreen Pear	Mix of 15", 24", 36" box
Schinus molle	California Pepper	Mix of 15", 24", 36" box

V.H PLANTING LIST: TREE, SHRUB AND GROWDCOVER ADJACENT TO BUILDING (continued)

<u>BOTANICAL NAME</u>	<u>COMMON NAME</u>	<u>MINIMUM SIZE</u>
<i>Shrubs</i>		
Azalea 'variety'	Azalea	5 gallon
Calliandra haematocephala	Pink Powder Puff	5 gallon
Camellia jap 'variety'	Camellia	5 gallon
Chamaedorea 'variety'	Bamboo Palm	5 gallon
Coleonemia pulchrum	Pink Diosma	5 gallon
Escallonia fradesi	Escallonia	5 gallon
Hemerocallis 'variety'	Day Lily	5 gallon
Heteromeles arbutifolia	Toyon	5 gallon
Ilex aquifolium "silvery"	Variegated English Holly	5 gallon
Liguistrum texanum	Texas Privet	5 gallon
Liriope 'variety'	Lily Turf	1 gallon

V.H PLANTING LIST: TREE, SHRUB AND GROWDCOVER ADJACENT TO BUILDING (continued)

<u>BOTANICAL NAME</u>	<u>COMMON NAME</u>	<u>MINIMUM SIZE</u>
<i>Shrubs (continued)</i>		
Nandina 'variety'	Sacred Bamboo	5 gallon
Phormium 'variety'	Flax	5 gallon
Photinia fraseri	Photina	5 gallon
Pittosporum 'variety'	Mock Orange	5 gallon
Podocarpus "maki"	Shrubby Yew Pine	5 gallon
Rhapliolepis 'variety'	Indian Hawthorn	5 gallon
Rosmarinus 'variety'	Rosemary	1 gallon
Rumohra adiantiformis	Leatherleaf Fern	5 gallon
Trachelospermum jasminoides	Star Jasmine	1 gallon
Tulbaghia violacea	Society Garlic	1 gallon
Xylosma congestum	Xylosma	5 gallon

V.H PLANTING LIST: TREE, SHRUB AND GROWDCOVER ADJACENT TO BUILDING (continued)

<u>BOTANICAL NAME</u>	<u>COMMON NAME</u>	<u>SIZE</u>
<i>Screening Shrubs</i>		
Escallonia fradesi	Escallonia	5 gallon
Ligustrum texanum	Texas Privet	5 gallon
Photinia fraseri	Photinia	5 gallon
Xylosma congestum	Xylosma	5 gallon
<i>Vines</i>		
Bougainvillea 'variety'	Bougainvella	5 gallon
Gelsemium sempervirens	Carolina Jessamine	5 gallon
<i>Groundcovers</i>		
Rosmarinus 'prostrattus'	Rosemary	1 gallon
Trachelospermum jasminoides	Star Jasmine	1 gallon

WALL DESIGN

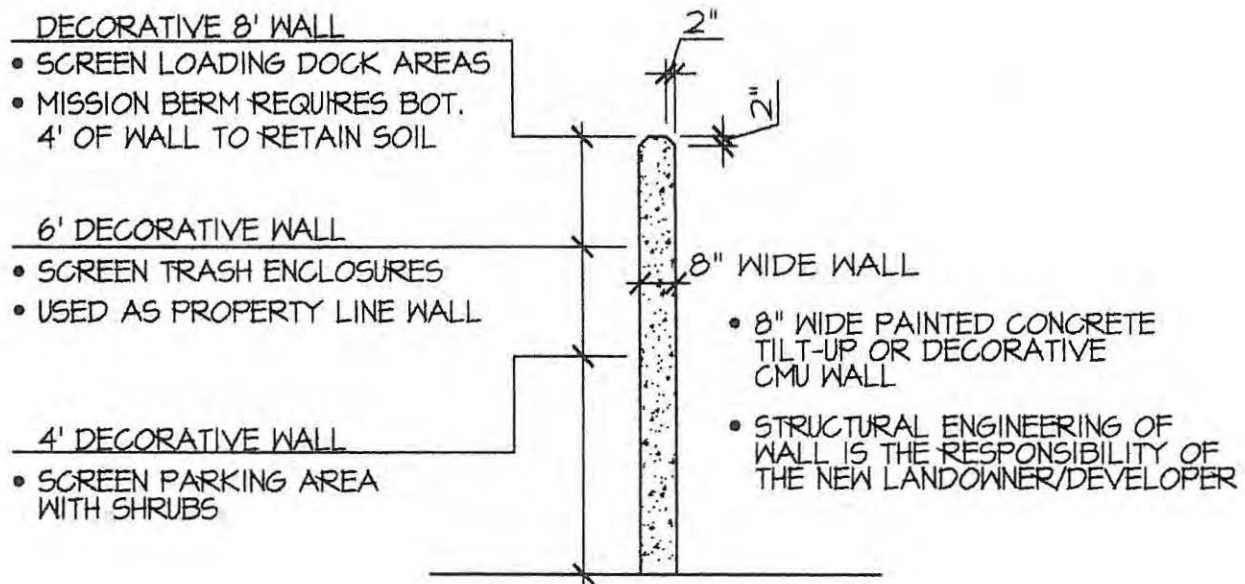


Exhibit 5-Q

V.I LIGHTING

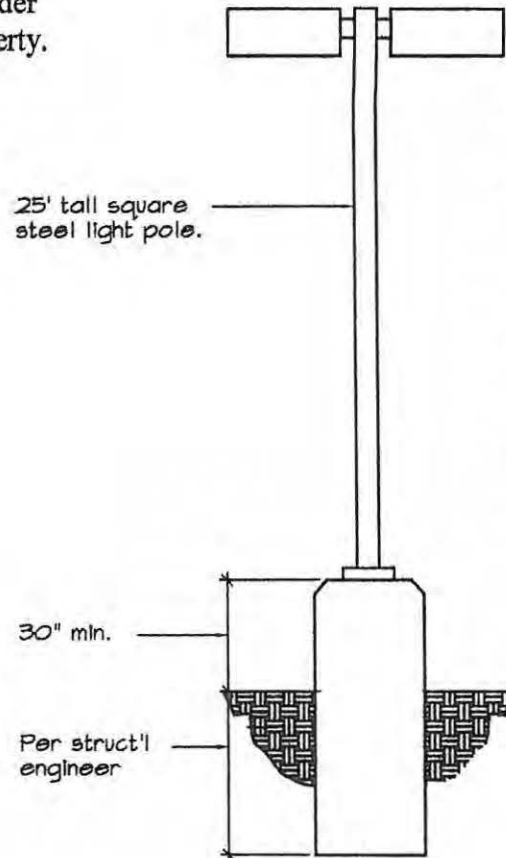
Promoting safety while creating an aesthetically pleasing environment makes lighting an integral part of sites. Universal design for the lighting has been chosen in order to keep uniformity and integrity from property to property.

Fixture (or equal as approved by the City of Pomona):

Total Lighting Concepts
733 East San Bernardino Road
Covina, California 91723
626-966-1611

Product (ordering information):

Kim Lighting
VL Series
Mounting: 2B
Fixture Size: 21"
Pole Size: 20'
Electrical Module: HPS
Finish: Black
Watts: Per City's Standards to meet required foot-candles in parking area.



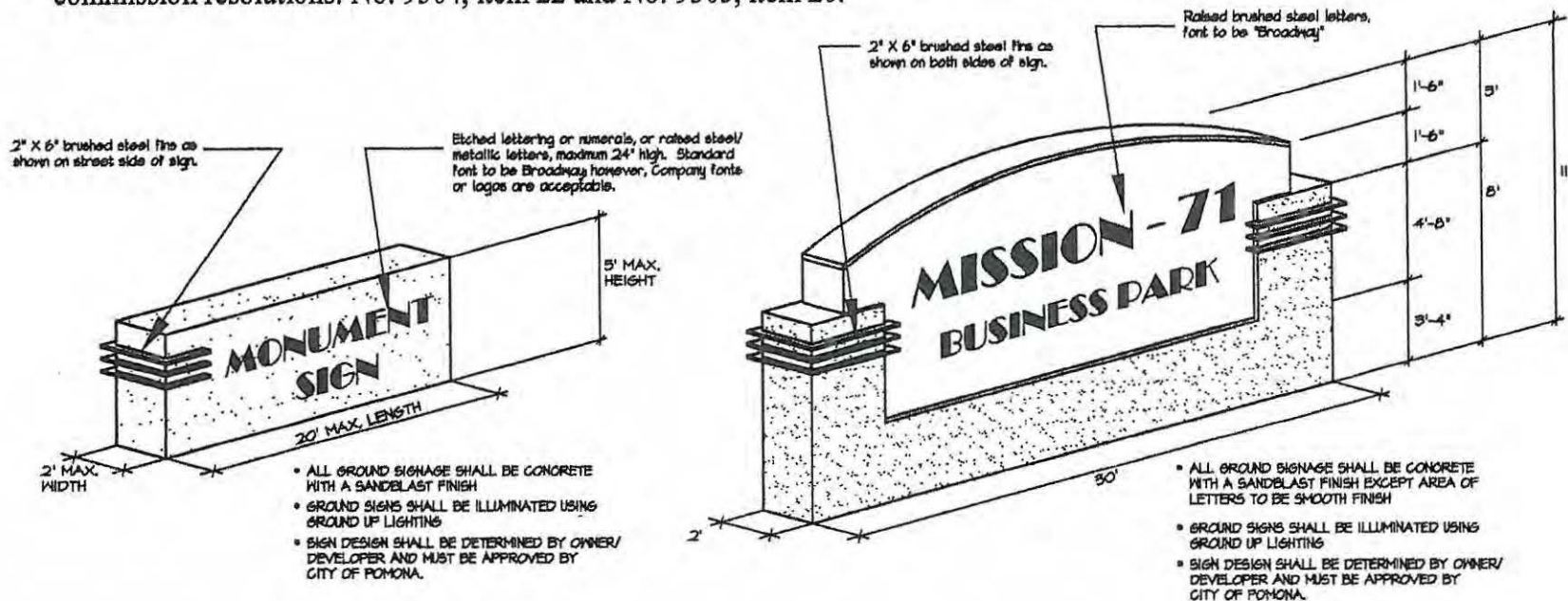
Provide 30" clear pole base above finished grade for car parking areas and 48" in truck yards.

Provide 24" diameter deep brushed or sand-blasted aggregate concrete pole base with 1" chamfer full circumference

Pole base shall be designed by a licensed structural engineer

V.J SIGNAGE

Signs shall be limited to business names, Owner's names and company logos. Exhibit 5-S outlines basic maximum dimensions allowed. Illuminated exterior signs are allowed, however, blinking, gyrating, rotating or moving signs are prohibited. Signs shall be of the size noted or such larger size as approved by the City of Pomona. A signage program will be approved by the City of Pomona per planning commission resolutions: No. 9304, item 22 and No. 9305, item 20.



Monument Signage

Project signage at entries from streets.

The City reserves the right to approve all signage, including identification, directional and traffic control signs.

Exhibit 5-S

SIGNAGE – SITE LOCATIONS

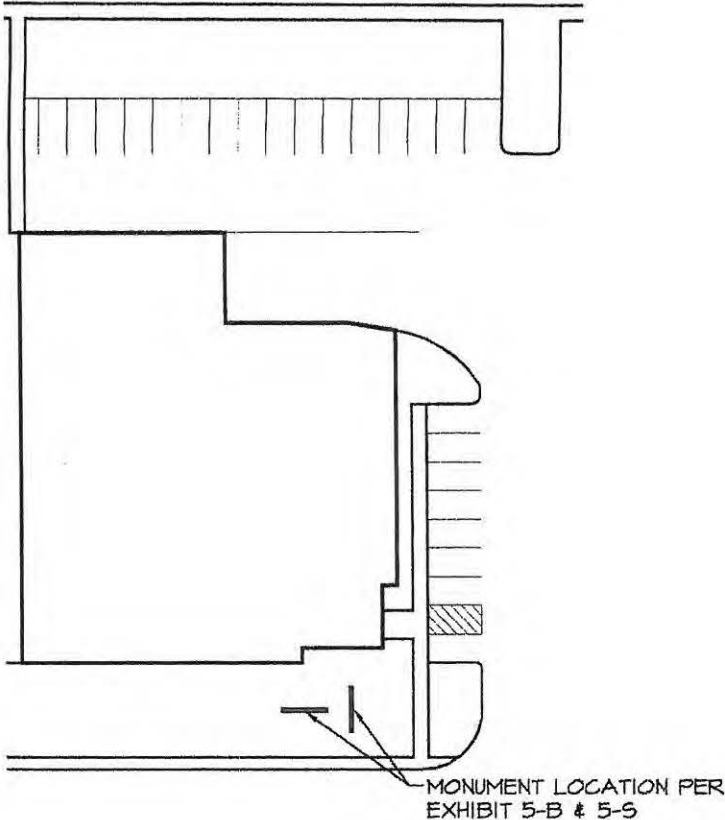


Exhibit 5-T

SIGNAGE – ELEVATIONS

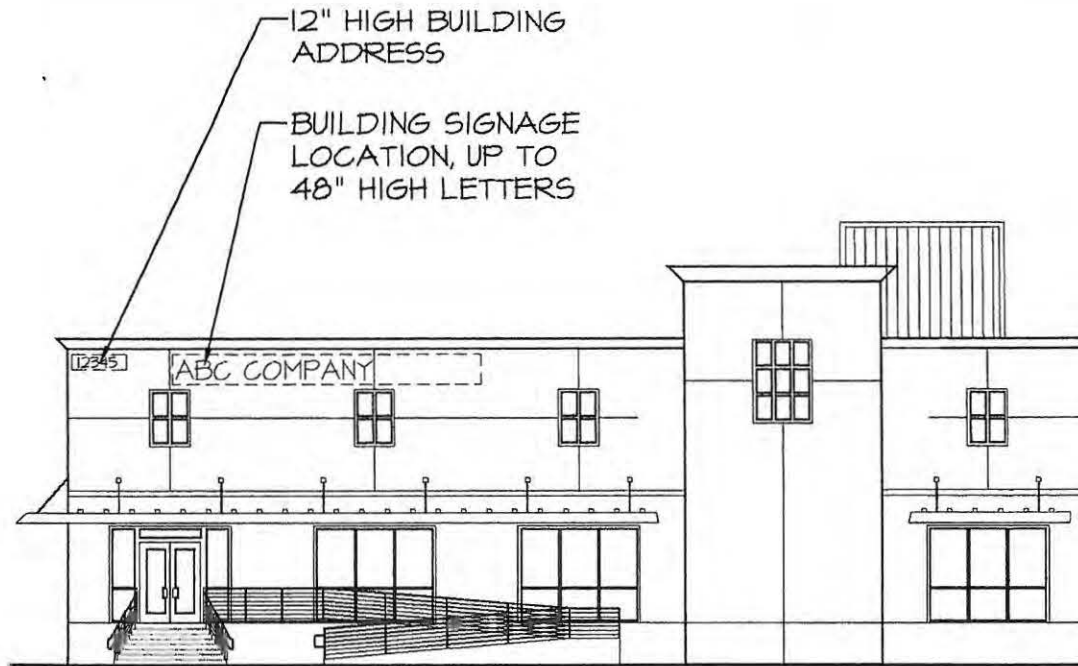


Exhibit 5-U

